

SPECIFICATION BOOK



2023/2024

DIRT SERIES

Motor racing is a sport – a dangerous sport – but a good sport is made up of men and women.

This is no place for the troublemaker or the person who can see nothing but wrong. Believing that every action of the officials is directed at them; that rules are made only for them to break. This person has no place in the sport of motor racing.

Glenn Perret

**Executive member Formula 500's
Australia**



FORWARD

Legend Cars Australia officials attempt to be fair and maintain consistency with the application of these specifications.

Our objectives are fairness and safety.

For an organisation to be successful, it must have good rules and enforce them fairly and consistently.

Cooperation between officials and competitors will assure our sport a bright future.

After a race car has passed scrutineering, it may not be altered to any specification that would make it illegal.

Any race car that has passed scrutineering may still be subject to further inspection at the discretion of scrutineers and may be excluded from the event and driver suspended.

Please be advised that all rules will be applied as per this specification book and the onus falls to the driver to ensure he is familiar with this rule book.

Please do not try and bring a race car that is out of 'spec', as you will be required to bring it back to 'spec' at the track before being permitted to compete.

Scrutineering is required to ensure all race cars are safe and evenly matched.

It is in YOUR OWN INTERESTS to present a LEGAL and SAFE race car.

Thank you.

Participant Code of Conduct

INEX members, Associate Members, and participants at INEX Events are always expected to conduct themselves in a professional and nondisruptive manner. Any member, Associate Member, or participant(s), who within the boundaries of the Track facility or otherwise, in the sole judgment of the INEX Competition Director:

- Physically assaults another participant, Official or other person.
- Engages in any conduct deemed by INEX in its sole and absolute discretion to be detrimental to the sport of, Legend Cars racing.
- Otherwise creates a condition or circumstance which is unsafe, unfair, or out of order, shall have violated the rules and regulations of INEX and may be penalized.

The Driver and/or Associate Member is responsible for the conduct of their crewmember(s) or anyone else associated with their team. A Driver may suffer the penalties for the conduct of their crewmember(s), Associate Member(s), or guest. When the Driver(s) are under 18 years of age the designated Associate Member (parent or guardian) is responsible for Driver, crewmember(s) and/or anyone associated with their Team. In this circumstance the penalties may still be imposed upon the Driver as well as the Associate Member. Physical violence violations may also be prosecuted through Local, State or Federal enforcement authorities.



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PART ONE

SAFETY EQUIPMENT

All items of Safety Apparel must meet the relevant standard as set down by the governing body level of licencing. It is the drivers responsibility to ensure adherence to all safety regulations at the time of any given event.

Drivers ***must*** wear a full-face helmet of current standard any time they are on track. NOTE: a helmet is designed to absorb energy once. If a significant impact has occurred to the helmet, it is highly recommended that it be sent back to the manufacturer for evaluation or replaced.

All safety apparel is to be SFI or FIA rated to meet current standards, dates and conditions. Fireproof underwear, socks, boots, gloves and balaclava are ***mandatory*** under all fire-retardant race suits, regardless of manufacturers guidelines. Double layer or more race suits are highly recommended. All items of safety apparel must meet the relevant standard which must be clearly identifiable on the item.

Head & Neck restraints are ***mandatory***. A Horse Collar may only be used for practice or 2-Seater laps.

Arm restraints are ***mandatory***.

The Legend Cars Australia Safety Grill is ***mandatory***.

All cars must be fitted with an approved 5 or 6 point racing harness mounted to the manufacturers mounting points or the equivalent to the scrutineer's satisfaction with minimum 8 mm diameter high grade bolts and nyloc nuts with the thread of the bolt protruding into the section of the nut.

SFI belts will have a 2 year life from stamped date as is the current ruling. Current 3 inch five point SFI latch link belts may be used as per stated date on the belts. All safety harnesses are to be no more than 2 years old from the date stamped on the SFI tag or as per expiry date on tag, regardless of how many times the car has been raced.

3 inch belts are to be used only with SFI 38.1 approved Head & Neck devices that are designed to accommodate 3 inch belts. SFI rated belts with 2 inch shoulder strap may be used with 2 and 3 inch SFI 38.1 Head & Neck devices.

Notwithstanding the date range, a restraining device must be suitable for service, as inspected at event scrutiny, at all times during competition. Any belts that are frayed, torn or in the opinion of the scrutineer, damaged, need to be replaced before competing. It is ***strongly recommended by*** the

manufacturers of Safety Harnesses that should the harness be involved in a major impact they be replaced immediately.

The seat belts are like the helmet usage suggestions, in that they are designed for protection in only one major impact. If you have had a significant incident, the manufacturer strongly recommends immediate replacement of the belts. Also, make sure that your harnesses are secured and that you have pulled the straps as tight as possible anytime you drive the car. Continue to retighten the harnesses under caution.

Drivers **MUST** install roll bar padding on sections of rollbars that may come into contact with the helmet in the event of an impact. Roll Bar padding is **mandatory** in all cars.

FIRE EXTINGUISHER

In Car - A 1.0 kg dry powder fire extinguisher is **optional**. All fire extinguishers must comply with all conditions as per the Governing Body. Fire extinguishers must be in date, tagged, and regularly inspected for currency. Fire Extinguishers must be fitted in front of seat under driver's legs and must be bolted in position with satisfactory clamping device. Race engineered pull cable fire bombs are to be inspected and signed off as approved in Logbooks. Heat activated fire bombs are not recommended.

In Pit Area – All teams are required to carry as a minimum in the pit area/transporters a 2.0kg Dry Chemical Powder Extinguisher or greater.

SEATS

Only factory manufactured metal or composite material seats approved by INEX or LCA may be used. **NO** homemade seats. Lightening of the seat by anyone other than the manufacturer is prohibited. **NO** alterations to framework bars are to be carried out to mount the seat. If altering the seat for safety and comfort the front seat mounting brackets (two of) may be modified. The modified mountings must still be welded to the floor seat mounting rail (this rail position cannot be changed). If the seat mountings are longer than the originals, then they should be suitably reinforced or made of heavier material in proportion to their length. The rear seat mountings cannot be altered. Seats are **not** to be directly mounted to the floor. All bolts must be a minimum 8mm (0.32in) bolts with minimum 35 mm (1.38 in) backing washers, with the thread of the bolt protruding into the section of the nut, on the seat to prevent bolts pulling through the seat in the event of an impact. No drilling through bar work allowed. All seat mountings are to be to the satisfaction of the presiding technical inspector.

PART TWO

LEGEND CAR SPECIFICATIONS/RULES

Upon entering a Legend Car for competition any car may be checked for engine, chassis, fuel, tyres, wheels, rear ends, suspension or any other component to confirm legality at any time by an official or representative of LCA.

The driver of the car is responsible for ensuring the legality of a car entering into competition and will be the person that will suffer all resulting penalties. Refusal to allow any part of a car to be inspected

by an official of LCA will result in immediate disqualification and possible suspension from future events in Legend Cars.

Drivers are responsible for the conduct of themselves, pit crew and associates involved with the driver and car. Any breach of misconduct in regard to abuse of another driver, official, pit crew or associates whether verbal or physical from the above mentioned during the course of a race meeting, will result in immediate disqualification from the event and further penalties will be imposed by LCA at a later date.

If this rule book does not specifically say that you can change, modify, add or remove something then you must consider that the change, modification, addition or removal of that item is ILLEGAL.

Any enquiries regarding the change, modification, addition or removal of any item of the race car must be submitted in writing to LCA with all supporting documentation.

LCA reserve the right to trial new components at their discretion.

DEFINITION OF TERMS USED

“LCA” – Legend Cars Australia, deemed the sole manufacturer of Legend Cars in Australia.

“INEX/LCA” – Interprets and enforces the specification rules for all Legend Cars.

“Stock” or “Original Equipment Manufacturer/OEM” as delivered “new” from U.S. Legend Cars International or at the current standards of US Legend Cars International or LCA.

“Disqualification” – unless otherwise defined will mean the forfeiture of all awards, prizes and points earned for and prior to the infraction during the date(s) of the event.

“Competition/Race” – A contest in which an INEX driver takes part, and which is of a competitive nature or is given a competitive nature by the publication of results.

“Event” – An entire program of on track, INEX or LCA competitions or races, including practice.

Every event which uses the name “Legend Car/s” or “INEX” or any likeness to the name or logo must be formally approved by INEX or LCA.

“Unauthorized” use of the name by events or people will be vigorously prosecuted to the fullest extent.

U.S. Legend Cars International (USLCI) and LCA when used in the current INEX Rulebook, will be considered the same entity.

COMPETITION RULES

All new and second-hand cars eligible for competition in Australia will be sourced through U.S. Legend Cars. VIN number, registration papers, and validity of cars will be checked by USLCI in the USA and upon arrival in Australia, will be checked and registered by LCA.

Logbooks will be supplied for all cars by Legend Cars Australia. Any driver who fails to produce the Logbook for his car at any given event will be ineligible to compete in that event. Logbooks will remain the property of LCA and we reserve the right to recall at any time.

All Legend Car drivers will be INEX members and will have a current membership number. These individuals will be eligible for National and International Championships.

All cars eligible for competition in Australia must be registered by LCA and have a Registration sticker valid from 1 July to 30 June each year as per Logbook. Current Logbook must be available to Scrutineers at all times. Logbooks are valid for 3 seasons. Logbooks are only available through LCA. Loss of Logbook will incur a \$100 replacement fee.

In the event of a damaged or painted over Registration sticker, LCA will allocate a new Registration number at no charge to the owner/driver. Annual Registration Inspections cover structural and engine compliance at that time only, ***all safety requirements for every race meeting remain the responsibility of the owner/driver.***

Cars not registered with LCA and not having a registration sticker approved by LCA will not be eligible to compete in Australia.

No Registered Legend Car will be permitted to compete on any track or at any event not sanctioned by Legend Cars Australia unless express permission has been granted by LCA. Any driver in breach of this rule will receive a minimum 12 month suspension with the immediate deregistration of their car/s and Logbooks returned to Legend Cars Australia within 10 business days.

The first calendar date marks the start of the season, and any events added to the existing calendar after this time will automatically be non-points rounds, unless there are special circumstances and approved by LCA.

Unless prior arrangements have been made, Legend Cars will not be permitted to race with full sized cars due to major safety concerns.

DRUG AND ALCOHOL POLICY

All Legend Car drivers are bound by Speedway Australia's Drug and Alcohol Policy. This Policy can be viewed at <https://www.speedwayaustralia.org/media.ashx/1157fd32-624a-4f82-96b3-4b22b6b7f969.pdf>

Random Drug and Alcohol testing may be carried out at any time. Refusal to undertake testing will result in immediate disqualification and loss of points for that round.

GENERAL

At each race meeting, cars will be self-scrutineered by the driver when setting up their pit area. The driver must complete the event information in his/her Logbook and sign off as fit to compete. The Logbook should then be left with the car for Track Scrutineers. Track scrutineering will be carried out at the discretion of the venue.

If further inspection is required on any area of a Legend Car, the driver will be required to remove or make that part accessible to the Scrutineer. Failure to comply may result in loss of points or disqualification for that event. LCA and their Scrutineers are not responsible for any issues arising from these inspections and the onus is on the driver to have the required skills/ability to perform any tasks required. Any driver who fails to attend scrutineering or a weigh in directly after a race, if requested, will face disqualification unless prior arrangements have been made with LCA. In addition, random spot checks by INEX/LCA may be performed. Logbooks must be presented on request or that car will not be permitted to race.

All personal safety gear as per the Governing Body Rule Book.

Only INEX or LCA stamped parts will be deemed legal unless otherwise stated.

A Logbook will be issued to ALL cars by LCA as part of Registration. Loss of this Logbook will incur a fee of \$100 for replacement.

IN-FIELD WORK AREA

If a track permits, there may be an in-field work area designated by the track officials/Chief Steward. Any works can be carried out during the allocated time, provided the car is deemed safe and secure by track officials, the Chief Steward and/or LCA officials **ONLY**. The work area is to allow cars to continue the current race, whether it be a heat or feature race. Any in-field repairs must then be rectified back to factory state prior to the next race in the event schedule. In-field work areas and repairs are at the discretion and approval of the Chief Steward and in-field officials.

ORIGINAL STARTS & RESTARTS

Once the pace car has exited the track, the front row cars must maintain Pace Vehicle speed and may not start racing until the restart zone / line (reviewed and defined in the Drivers Meeting).

All cars are to stay double-file, side by side in nose-to-tail formation without bumping or pushing. Once the Green flag has been displayed, you may pass (how and where, to be reviewed in the Drivers Meeting), penalties are at the judgement of the Race Director/Chief Steward. The pole sitter may choose the high or low line.

If a re-start is required with 5 or less laps remaining, it will be single file.

Re-starts may be single file at the discretion of the Race Director/Chief Steward if they deem the track unsuitable or unsafe for double-file re-starts.

PROTEST – RACING RESULTS

Any and all protests regarding racing results or decisions made by the track officials/Chief Steward are to be lodged as per Speedway Australia F500 process. This process can be found online at <https://www.speedwayaustralia.org/rules> Refer Appendix 5, 7.8.

PROTEST – PARTS & COMPONENTS

The purpose of the protest is to provide a safety valve to help keep the series competitive and to keep the cost of racing affordable. Only current INEX members may protest a component. The Protesting member must show their membership card to submit a protest. Any specific engine, rear end, or suspension components of a competitor's car finishing in front of the protestor, may be protested by a Driver and/or Associate Member finishing on the same lap or next respective finishing position of that feature after the Race for the amount designated on the protest fee schedule, payable in cash only.

Protesting back is considered spiteful & is not permitted.

On-track Race procedure calls cannot be protested to INEX/LCA.

Filming or recording of Protest Inspections or close-up photos of Technical INSPECTION AREA is not allowed.

TIME OF THE PROTEST: The protester's membership card, protest card and full payment in cash only must be presented to the INEX/LCA Technical Inspector/Scrutineer no later than fifteen (15) minutes from the arrival of the winning car into Inspection Area. The head Technical Official will determine time. Once a protest is submitted, it may not be withdrawn. All purse and points will be distributed after a decision on the protest has been made.

VERIFICATION OF PROTEST: The protested component will be removed from the car or made available for inspection by the protested team's crew and inspected by the INEX/LCA Technical Inspector/Scrutineer. The following people will be permitted to be present at the Inspection location at the racetrack: protested Driver and/or Associate Member and crew (the crew shall leave once the part is removed), protester, INEX/LCA Technical Officials/Scrutineers and a Track management representative. If a decision at the Track cannot be made, then the component will be sent to the INEX/LCA Technical Director for his sole decision on legality. On a protest, it is the Driver and/or Associate Member's duty to prove his legality, not the Technical Officials/scrutineers or protester's duty to prove the violation.

WHEN THE PROTEST FINDS A LEGAL COMPONENT: After inspection, if the protested component/part is determined to be legal, the protest money will be given to the Driver of the car from which the component was protested (less reasonable inspection fees and shipping charges if shipped to INEX. Inspection fees & shipping charges will be determined by the INEX/LCA Technical Director).

WHEN THE PROTEST FINDS AN ILLEGAL COMPONENT: After inspection, if the protested component is found illegal, the Driver of that car will be Disqualified from that Event, forfeits all prizes and honours earned at the Event and face the possibility of more penalties at the discretion of INEX/LCA Competition Director. The Associate Member will be advised, when applicable. The illegal parts will also be confiscated by INEX/LCA Technical Inspector/scrutineer. The protest money will be returned to the team filing the protest. The Driver and/or Associate Member who was found illegal will be responsible to pay for inspection fees and shipping charges in the form of a fine.

A car may only be protested by one other competitor at each Event. (If more than one protest is filed against a car, the Driver and/or Associate member with the higher finishing position in that Race will be allowed the protest).

No Driver may file a protest more than 3 times during the calendar year.

Protestor must verify that the INEX/LCA Technical Inspector/scrutineer has the special equipment necessary for the test, if applicable.

REFUSAL OF A PROTEST: If the Driver and/or Associate Member of the protested car does not allow inspection under these terms, the Driver will be Disqualified, forfeit all points, purse monies, prizes and awards earned at that Event and possibly suffer more penalties as determined by INEX or LCA. The car will also be suspended from Competition.

NON-ACCEPTANCE OF A PROTEST by INEX: The protest may, at the discretion of the INEX or LCA Official in charge, be declined if deemed to be malicious or spiteful. INEX/LCA Competition Director may also penalize the author of a protest deemed to be malicious, spiteful, or who otherwise acts in bad faith.

PROTEST FEE SCHEDULE – PARTS & COMPONENTS

Limited to the choice of one (1) of the following to be protested:

\$100:

- Compression Ratio (Choice of any 2 cylinders)
- Bore Size (Check all 4 cylinders with bore gauge)
- Stroke
- Ported & Polished Head (check intake & exhaust)
- Air Filters (Velocity Stacks and size of filter)
- Cam Shaft Lobes (check intake & exhaust lobes)
- Alternator (working & wired properly)
- Exhaust System (check header and muffler)
- Total Weight (minimum weight)-Right Weight - Rear Weight Percentage
- Rear End Gears
- Shocks (each shock)
- Spindle (Left & Right)
- Driveshaft (Stock)
- Wheel Offset (Check all four wheels)
- Bumper (Front or Rear) meets all specifications.
- Carburetors
- Durometer test of tires
- Ignition box
- Engine Seals (Removed in the field by the INEX Inspector and sent to INEX headquarters for inspection. Only if INEX Inspector has replacement seals for re-install).

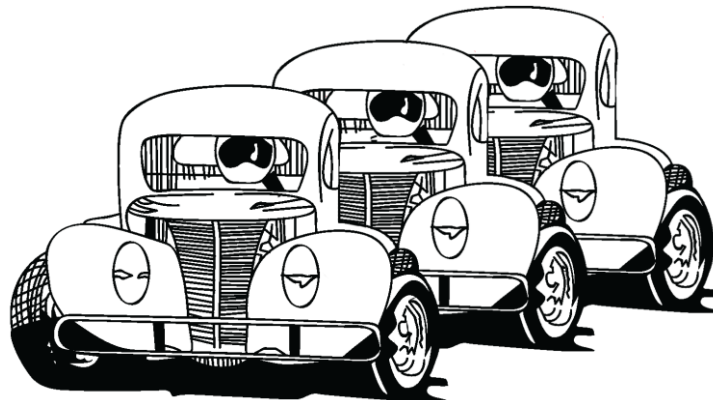
\$250

- Fuel (Laboratory Test)

\$1000.00

- Engine Inspection (Carburetors, Alternator, and Starter will be INCLUDED). **NOT** included: ignition electrical components, ignition box, coils, header, oil cooler, remote filter, sprocket adaptor, clutch slave cylinder, and air ducts.

TBD -Reasonable fee to be determined by the INEX Official for all other specific items.



PART THREE

INDEX

1. AERODYNAMICS

Spoilers or any other devices that affect aerodynamics are not permitted.

2. AIR FILTERS

Only stock air filters, dual K&N air filters or single air filters and Outerwear's Pre-Filters coverings are permitted. Velocity/Ram Stacks are not permitted. No components that direct airflow to the carburetors or increase airflow are permitted.

FZ09 and MT09 must use LCA supplied air filters.

3. ALTERNATOR

Modification(s) to the charging system is **NOT** permitted. The alternator system must be charging at all times. No switches that disconnect the alternator from the charging system are permitted. The engine must continue to run at idle with the battery disconnected.

4. BALL JOINTS

The upper and lower ball joint must remain Stock, within the Stock dimensions, steel thickness, location and configurations supplied by LCA and may not be reinforced in any way. One or two (minimum ¼", maximum 7/16" width) jam nuts are permitted on the upper and lower ball joints. Each ball joint must use a minimum of one jam nut. Jam nut(s) may not be welded to control arms. Limiting the natural travel of the ball joint is **NOT** permitted. Additional split pins or R clips are acceptable as nut locking devices.

5. BALLAST

A maximum of eight (8) blocks of solid lead are permitted on the car. The blocks must be no larger than 38mm (1.5") x 63.5mm (2.5") x 305mm (12") (approximately 20lbs or 9kg). Stacking of blocks of any size is **NOT** permitted. All ballast must be painted in a bright colour and identified with the car number. Ballast must be lead only and may not be added by any other method, including steel shot or any other material in the frame rails, bumpers, nerf bars or any other component.

Mounting of Ballast - The ballast blocks must be bolted directly to the sub-frame and must be secured with a minimum of two (2) 2mm x 8mm bolts per block. Ballast may be bolted to the square tubing of the sub-frame from the front to the back (including the cross members) (1" X 2" tubing only), not to the roll cage. The blocks must not be encased in any way. The ballast must maintain a minimum of 3 ½" inches of ground clearance. Ballast must not extend past the front frame horns or rear bumper mounts or extend beyond the subframe at the kick ups. Ballast is **NOT** permitted to be mounted inside the driver's compartment. No ballast may

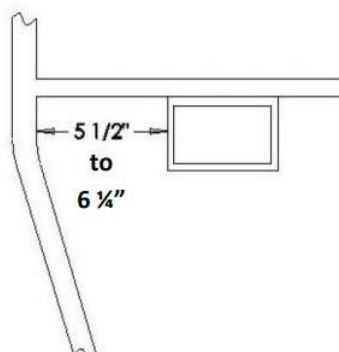
be bolted to the running board or the nerf bars. Ballast may not extend laterally, beyond 2 ½" from the outside of the frame rail. All mounting of ballast is subject to approval by scrutineers.

6. **BATTERY**

Only one battery is permitted per car: acid, gel or the LCA approved Lithiumax RACE9+ LCD battery (as delivered by LCA, Simpson Toorace Australia or as purchased from a company that stocks this **exact** battery). The USLCI Feather-Lite or ANTIGRAVITY ATX-30 battery as delivered by USLCI are also permitted for use. A new cradle insert **MUST** be purchased from LCA or USLCI if the lithium battery is chosen (shown below). All new cars from USLCI/LCA will come supplied with the new cradle. Approximate dimensions are 7 1/8" x 8" x 6 ½" (18cm x 20.3cm x 16.5cm) **OR** 16.5cm x 17.5cm x 12.5cm for the Lithiumax Race9+. Top or side post types may be used. Ensure the battery is adequately secured and use the correct cradle to match the battery type. Homemade cradles are **NOT** permitted.

Motorcycle batteries are **NOT** permitted. The battery must remain in its Stock location and securely mounted. The original battery cradle and bracket must not be altered in any way. Starter solenoid must be mounted on the battery bracket when using the 1250 engine. Terminal covers or rubber padding around the rear end is highly recommended. A battery isolator switch is optional and if used, **must be** labelled ON & OFF.

Legends Battery Box Offset



Note: The measurement for the battery box offset will be taken at the point closest to the crossmember, from the inside of the frame rail to the outside of the battery box.

7. **BOLTS AND FASTENERS**

Only equivalent Stock or upgraded steel fasteners and bolts may be used on Legend Cars. Fasteners may be drilled for safety wires, however intentional weight saving modifications are not permitted. All bolts are to be magnetic. Aluminium and titanium or composite material bolts are not permitted.

Rear Trailing Arm Chassis Mount - A **mandatory** 2 ¾" ½" bolt is to be used in conjunction with a ½" half nut on the Rear Trailing Arm Chassis mount.



8. BRAKES

Any of the brake parts that are attached to the rear end or the spindles must remain Stock, within the Stock dimensions, steel thickness, location, and configurations. Willwood brake calipers are permitted as delivered by LCA. Brake calipers must be mounted on the back side of the rotors. The car must have operational brakes on all four wheels and must lock up all four wheels during inspections (Brake lines may not be plugged or shut off).

Any type of brake cooling duct is **NOT** permitted.

The right or left side brake pedal may be removed.

Brake line quick disconnects are **NOT** permitted.

Complete elimination of the brake at any one wheel is **PROHIBITED**.

Any car found to be in breach of this rule will incur an automatic and immediate suspension.

Only Willwood, Mintex, Ferodo or Carbotech brake pads or shoes are permitted. Steel brake lines/"cupro nickel lines" may be substituted for steel braided brake lines and re-routed to give greater protection. No rubber or copper brake lines are permitted. The new Willwood brake system may be used and must carry the INEX logo. Only Willwood or Cobalt pads supplied by LCA may be used.

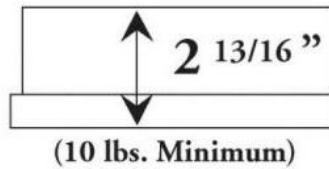
Brake Rotors:

Only steel rotors (**NOT drilled** or reduced in diameter) are permitted on the front as delivered by USCLI/LCA. The minimum thickness of a front brake rotor is 8mm.

Brake Drums:

Only Stock steel drums (not drilled or lightened) are permitted on the rear. Rear disc brakes are **NOT PERMITTED**. The minimum weight of the brake drum is 10.0 lbs (4.53 kg). Removal of metal from the brake drum for lightening purposes or "offset" is not permitted. Only the "shoe face" may be machined.

Legends Brake Drum



Brake & Clutch Lines:

Rubber, hardline or steel-braided brake and clutch lines are permitted. No brake lines may be disconnected or plugged.

Brake Master Cylinder:

The brake master cylinder must remain Stock, within the Stock dimensions and location configurations as supplied by LCA. The brake master cylinder must remain on the engine side of the firewall. Aftermarket remote reservoirs are permitted.

Brake Proportioning Valve:

Any number of aftermarket brake proportioning valves for the brakes are permitted, but complete elimination of the brake at any wheel is prohibited.

9. **BUMP STEER**

Adjustments to the bump steer settings will only be permitted by placing spacers between the steering rack and the heim joint ends of the tie rods or between the front spindle pick-up points and the heim joint ends of the tie rods. No other modifications to adjust bump steer such as changing the height of the steering rack or modifying the spindles are permitted.

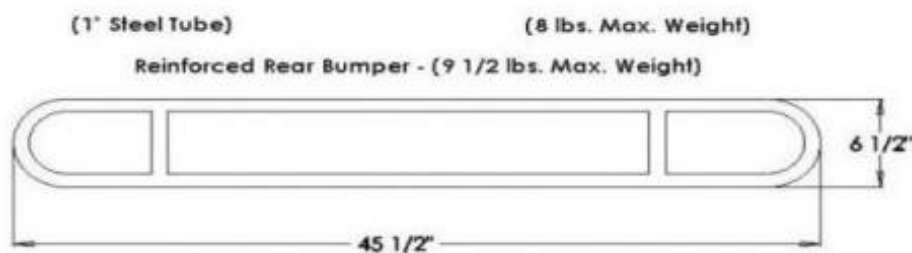
10. **BUMPERS AND TOW HOOKS**

Front Bumper:

The front bumper must remain within the Stock dimensions, steel thickness, location, and configurations and must not be reinforced, except for adding one (1) additional bolt per front and/or rear frame rail to the tab.

Bumper Safety Straps are mandatory.

Legends Front & Rear Bumper



Rear Bumper:

The rear bumper must remain within the Stock dimensions, steel thickness, location, and configurations. The rear bumper may be reinforced by attaching tubular steel from the bumper to the rear frame horns. One (1) additional bolt per frame rail to the tab is permitted. If reinforced, the steel tubing (12" maximum length) may not extend beyond the outside width of the rear bumper or attach to the rear cross member of the frame.

Bumper Safety Straps are **MANDATORY**.

Attachment of the Bumper:

Bumper tabs must use a minimum of one bolt per tab securing the bumper. There must be a minimum of 1" from the end of the frame rail to the back of the bumper upright before a Race begins. Should the tab become bent during a Race, it must be fixed before the next time the car goes on the track to compete. Tie-wraps, safety wire, duct tape, etc., are not permitted to secure the front or rear bumpers or bumper tabs.

330mm straps must be fitted. M12 tow hooks must be mounted at left and right hand front chassis rail and left and right hand rear chassis rail (**NOT** through bumper tabs).

If a bumper (front or rear) becomes dislodged whilst on track, the competitor must enter the in-field work area for the bumper to be re-attached (to the satisfaction of the scrutineer/in-field official/chief steward/race director) or a replacement bumper is to be fitted. It is **NOT** permitted to re-join the track without a bumper.

Bumper Tabs:

The bumper tabs must be Stock. And must be .125" - .140" in thickness. Tabs with excessive weld will not be permitted.

Repairing a bumper During a Race Meeting:

Bumpers are mandatory throughout the Race and must be bolted on in approved manner. Tie-wraps, safety wire, duct tape, etc., are not permitted to repair the bumper(s).

NOTE: Bumpers are mandatory. A car must compete with both bumpers that are not in contact with the track or considered in a dangerous position or condition by Track Officials at any time. If any of these conditions are not met, Track Officials may black flag the car.

11. BUMPER SAFETY STRAPS

All bumper bars are to be fitted with two seat belt straps.

The straps are 330mm seat belt straps with eyelets at each end.

The front straps must be mounted to the lower grill mounting points. They must not be mounted to the front bumper bolts or tow hook bolts. Two additional 3/8" (10mm) bolts are permitted to be added to the left and right hand side of the front clip to secure straps if room allows. See alternate position for grill mount below.

The rear bumper straps must be mounted to the left and right hand tow hook mounting bolts. They must not be mounted to the rear bumper bolts. See photograph 5 below.

BUMPER SAFETY STRAPS

Bumper Safety Straps are to be seatbelt extensions with eyelets, measuring 13" (330mm). They must be mounted as per the photographs below.

On the front, the straps are to be mounted using the lower grill mounting points. Bolts may be increased to 3/8" (10mm). Two additional 3/8" (10mm) bolts are permitted to be added to the front, on the left and right hand side of the front clip to secure straps if room allows. This ensures the straps will hold the bumper high enough off the ground to prevent it being caught under the front wheels.



At the rear of the car, the straps must be mounted using the tow hook mounting bolts. They must not be mounted to the rear bumper bolts.



Under NO circumstances are any tow hooks to be fitted through any bumper tabs.

12. CARBON FIBRE

Carbon Fibre component usage is **NOT** permitted.

13. CARBURETORS

1200/1250 CARBURETORS: The carburetors and components of the carburetors must remain as Stock Yamaha FJ1200/ XJ1200/XJR1250 (sealed). Only carburetor jets, needles, slide springs and butterfly screws may be replaced. Butterfly screws may only be replaced with 3mm X 6mm Allen head screws or Stock screws. These screws must not be altered in any way other than “stamping” the end to secure the screw. All Stock adjusters may be used. No other modifications to the carburetors or components of the carburetor are permitted.

Modifications or components to increase or restrict airflow or fuel flow (such as velocity stacks, heat deflector shields, internal modifications not listed above, etc.) are not permitted.

14. CAR/DOOR NUMBERS

Cars shall have door numbers on both sides of the car that are minimum height of 400mm and minimum width of 100mm, or to the satisfaction of LCA with written permission.

Any such written permission must be noted in the Logbook by LCA officials and must be available to scrutineers at any time.

Roof numbers are permitted on the roof surface but are not compulsory. All cars must have the car number visible on the front and rear of the car, minimum height 100mm, minimum width 25mm. These numbers are to be placed on the Front Right guard and the Rear Left guard.

Car numbers will be allocated a State or Territory suffix. “A” for ACT Region (Braidwood, Marulan, Yass, Cooma circle), “N” for NSW, “NT” for Northern Territory, “Q” for Queensland, “S” for South Australia, “T” for Tasmania, “V” for Victoria, “W” for Western Australia. The suffix “NZ” will be used for New Zealand. Suffix lettering must be a minimum height of 100mm.

Competitors must apply in writing to LCA for allocation of a number and LCA will confirm the allocation to that competitor in writing.

Numbers will remain the property of LCA and cannot be sold privately. Numbers will not be transferable between competitors without LCA consent. LCA has the final say on number allocation. A holding fee of \$50 per year is to be paid to LCA to hold a particular number for a maximum of 1 race season. If you do not own a car, your number will be forfeit after one month.

A waiting list will be maintained by LCA and anytime a number becomes available, the first name on the list with that number as their preference will have first option on that number.

Please contact LCA for allocation of numbers.

Legend Car # 1AU shall be allocated each year to the Australian Title holder from the previous season. State Title winners may run #1 (e.g. #1N, #1Q, #1W) for that state for the following season should they desire, but this is not mandatory. All other numbers 2 – 99 may be used in each State or Territory and New Zealand.

All cars will run a small sticker on the right rear guard, to identify their INEX division to other drivers. (see **Driver Divisional Structure** section in **Part Six** below).



15. CHROME PLATING AND POLISHING

External parts such as bumpers, Nerf bars, suspension components and cam cover may be chrome plated or polished.

16. CLUTCH MASTER CYLINDER

The clutch master cylinder must remain Stock, within the Stock dimensions, location and configurations and the clutch master cylinder and reservoir must remain on the engine side of the firewall. No Aftermarket clutch master cylinders are permitted. The clutch pedal length may be shortened for Driver comfort.

17. COILS, COIL LEADS AND SPARK PLUG BOOTS

These components must be stock Yamaha parts to suit the engine used, stock grey INEX coil wires or any 8mm aftermarket coil wire (EG Bosch, Eagle etc.) or red Dynatek coils marked with the INEX logo as supplied by USLCI and stamped by INEX or LCA.

The spark plugs may be aftermarket type with the same thread size. The stock coil mount must not be altered in any way and may only be replaced with an INEX or LCA approved coil mount. Resistors must remain in spark plug wire ends.

18. DOOR PLATES

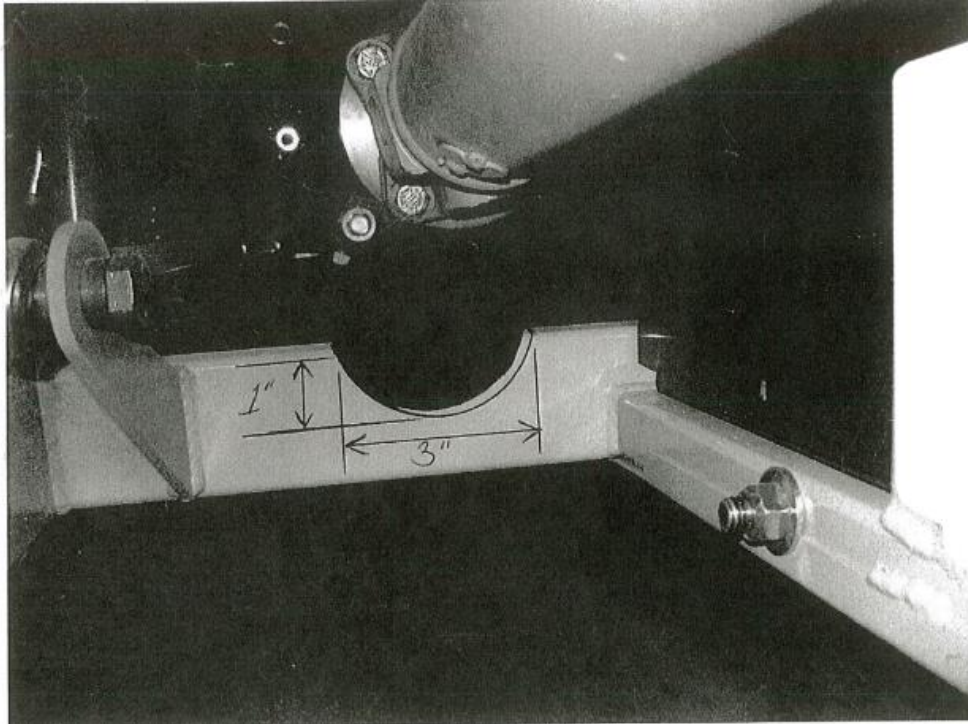
The use of a door plate on the Driver's side door is mandatory. Doorplates and strike plates must meet specifications of USLCI. The door plate is permitted to be added to the right-side door. A competitor may use a doorplate covering the entire door area on either side of the car (maximum of 1/8" thick).

19. DRIVESHAFT

The driveshaft, flanges, and U-joints and all components of the driveshaft must remain within the Stock dimensions, steel thickness, location, weight (14 lbs. minimum (6.35 kg)) and configurations of USLCI. The driveshaft must be painted white or light grey.

Driveshaft Retainer - INEX-approved driveshaft retainer strap is permitted. A maximum of three (3) retainers of 1/4" thickness and 3/4" width is permitted. Requests for additional retainers for the driveshaft must be submitted to and approved by the INEX Technical Director/LCA in writing.

Driveshaft Cutout - A "cutout" is permitted in the crossmember under the rear universal joint of the driveshaft. This modification will be permitted only as described and pictured below. The "cutout" will be directly below the centreline of the driveshaft and will be no more than 3" wide and no more than 1" (deep) into the 1" x 2" square tubing. This "cutout" must be plated with .083" steel (minimum). This modification (fitment and welding) is subject to final approval of the technical inspector.



20. ENGINE COATINGS

Only engine coatings as delivered Stock from the factory are permitted. Removal of any engine coatings is not permitted. Headers may be painted with high heat paint only. Header wrap tape is permitted. Repainting the outside of the engine or using an unpainted engine is permitted.

21. ENGINE COOLING

1200/1250 - Additional fan(s), internal duct work, hood louvers, remote oil filter, header wrap, holes in the hood, holes in the front fenders and/or oil coolers are permitted. The maximum height for louvers is 3/8" (9.53 mm). The size of the area for louvers or holes in the hood may not exceed 54 square inches (9" x 6") (229 mm x 152 mm). These components may not direct air to the carburetors or air filters.

Air scoops on bonnets are permitted. Scoops must not exceed 1.5" (38.1mm) in height, 10.5" (266.7mm) in width and 16.5" (419mm) in length.

SAAS Bonnet scoops are recommended.

22. ENGINE SERIAL NUMBERS

All engines must carry a serial number on the casing. Any engine without a serial number must be inspected and stamped by LCA prior to being sealed.

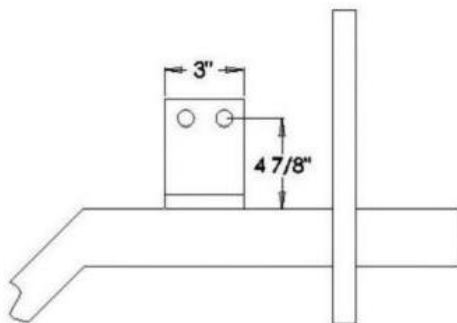
23. ENGINE LOCATION AND MOUNTS

1200/1250: Left and right-side engine mounts must remain within the Stock dimensions, steel thickness (no aluminium mounts), location and configurations. The right-side motor mount may be replaced with the optional motor mount (if the optional right-side mount is used; you may remove the Stock right side mounting tabs). Rubber mounts will be permitted if the engine remains in the Stock location.

The engine mounts must be bolted on to the frame as specified by USLCI.

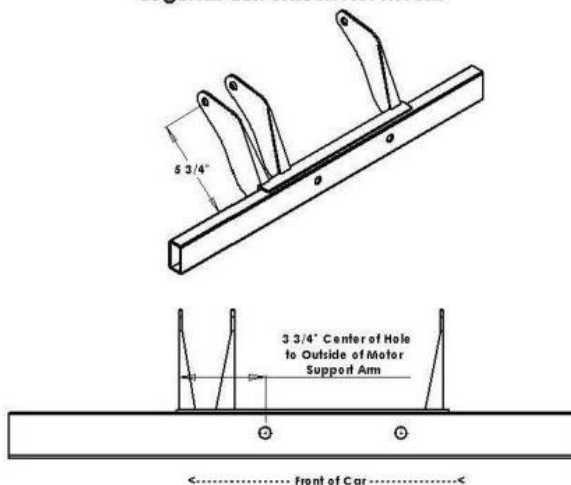
Mounts may not be welded directly to the frame.

Legends Right Side Motor Mount

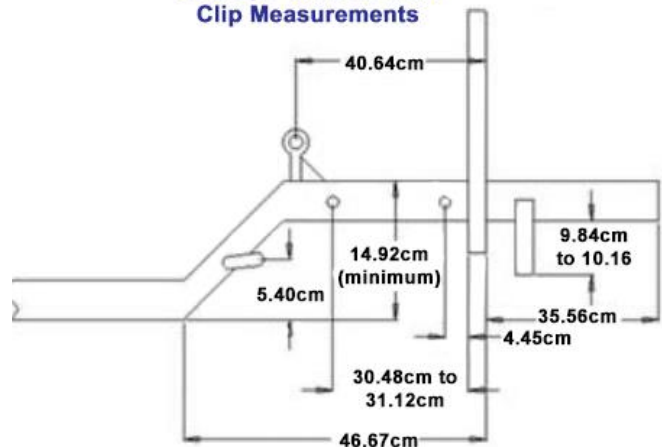


Aluminum mounts or extra holes are illegal.

Legends Left Side Motor Mount



LEFT SIDE MOTOR MOUNT Pick Up Points & Front Clip Measurements



24. ENGINES & SEALING

The only engines permitted are Yamaha:

FZ09

MT09 (approved by LCA or INEX)

XJR 1300

XJ 1250

XJ 1219

XJ R1200

FJ 1200

FJ 1100

Only two (2) oversizes for XJR 1250/XJR 1300 Sealed Engine + 4Thou and +8Thou.

All engines are to be checked and sealed. All engines will be measured and sealed by LCA prior to racing or tech sealed in the interim at the discretion of LCA or Officials. One week's notice is to be given to check and reseal the engine at a cost of \$100 to the owner. All engines are to be brought to the applicable engine sealer in their applicable state for testing and resealing – contact admin@legendcarsaustralia.com.au for authorised engine sealers for your area.

LCA reserve the right to install a Tech Seal at the track if necessary. The engine should be inspected and sealed prior to the next round. If the engine is deemed non-compliant at that time, all points for Tech sealed rounds will be lost.

Engines not sealed by LCA will be disqualified from the event.

FZ09 and MT09 **MUST** be supplied by LCA **ONLY**. Engine and gearbox must remain stock as supplied by LCA.

25. EXHAUST SYSTEM

The headers, muffler and gaskets must remain within the Stock dimensions, steel thickness, location, and configurations as supplied by LCA and stamped by INEX or LCA. Extra tabs, safety wire, sprints or chains, etc associated with the retention of the muffler are permitted for safety reasons. Mufflers may not be welded to the pipes. The internal components of the header and muffler may not be altered in any way.

Header wrapping (tape) is permitted.

Stock, INEX approved LCA Borla and S&S exhaust systems are mandatory for use with Yamaha 1200/1250.

The ProFab long exhaust system and quiet muffler as supplied by LCA is **MANDATORY** for use with Yamaha FZ09/MT09.

The muffler must have restraints fitted to prevent the muffler becoming dislodged during the racing. See **50. MUFFLERS**

26. FENDER MOUNTING

Stock fender mounts must remain and may not be altered. No additional fender supports or mounts are permitted. All '37 Ford style grills must use the inner mounting hole closest to the grill. Cars that use a Fibreglass grill shell may remove the original fender brackets.

Dzus fasteners may be used for fender removal purposes only.

Area under the Rear Fenders - Removal of the fibreglass panel on the body half under the rear fenders is permitted for access to the shocks, springs and rear compartment provided it complies with rule **39. General Appearance of the Car.**

27. FENDER HOLES AND TRIMMING

Holes:

Maximum of ten (10) hole(s) per fender. Maximum hole(s) is four (4) inches in diameter. Any shaped hole(s) equal to or less than the area of a 4" round hole is permitted.

Note: a four-inch hole saw will produce a hole larger than four inches.

Trimming of Fenders:

Front Fenders - The only trimming of the front fenders allowed is on the inside edge of the fender starting 12" above the frame rail on the front clip, along the contour of the fender, and only trimmed in 3" from the edge, back to the firewall. The inside of the fenders near the grill, the edge of the fenders near the body shell (past the firewall), the outside of the front fenders and the bottom edge of the front of the fender may not be trimmed.

Rear Fenders - Rear fenders may be trimmed **above the tire** for additional clearance. The rear fender contour must measure at least nine (9) inches from the main body shell. Measurements will be taken at the point on either rear fender above top dead centre of the tire. No holes or other trimming is permitted in the rear fenders or body shell.

28. FIBREGLASS COMPONENTS

All fiberglass body components must remain within the Stock dimensions, thickness, location, and configurations. All fiberglass components must have an authentic INEX certificate embedded into the underside of the fiberglass component, evidencing that the component is a certified USCLI part. Fiberglass components may not be reinforced or lightened in any manner. The use of a complete front end (Matching hood, fenders, grill, and grill shell) on a different model car is acceptable. All cars may use 34 Ford rear fenders.

Mounting of Body Components:

All fiberglass body components must be firmly attached to the Legend Car competing in any Race. It is recommended that all Dzus fasteners fit tight and are taped over to prevent loosening.

Rear of body may be trimmed between the frame rails up to the top edge of the rear frame horns. Body may be riveted along the side of the sub-frame if the original body mounts are not higher than the top of the sub-frame.

Removal of undamaged fiberglass body components (hood, deck lid, etc.) during an Event is not permitted. Any car may be black flagged or denied entry to start a Race if any of the aforementioned items are not attached, unless prior approval is given by an LCA Official.

Fibreglass components **must** be purchased from LCA or authorised LCA dealers.

29. FIREWALL

An aluminium firewall is mandatory. Firewall must be configured as delivered LCA. Using a "thicker than stock" aluminium firewall separating the driver's compartment from the engine compartment is permitted.

30. FRAME

Absolutely **NO** modifications of the frame or roll cage are permitted unless otherwise noted in this Rulebook. All frames must have a valid VIN plate secured on the frame. No weight reduction of the frame (including roll cage) is permitted. Only frames produced by USLCI and stamped by INEX or LCA may be used.

Any special requests or unusual repairs must be submitted in writing to LCA and, if granted, approval for such requests will be given in writing and must be kept available to officials at any time during a race event.

As of January 1, 2000, all Legend Car frames (coupes & sedans) with 0.065" E.R.W. main roll cage will no longer be eligible for competition in the Legend Car Series.

All frames must be replaced with a 0.083" D.O.M. main roll cage frame. It is estimated that all frames produced before August 1995 do not meet the above 0.083" D.O.M. requirements.

Suicide Doors - Doors with the latch on the A-Pillar and hinges on the B-Pillar rather than the traditional method of the latch on the B-Pillar and the hinges on the A-Pillar. Suicide doors are only permitted on the 1934 Ford and Chevy Coupe frame.

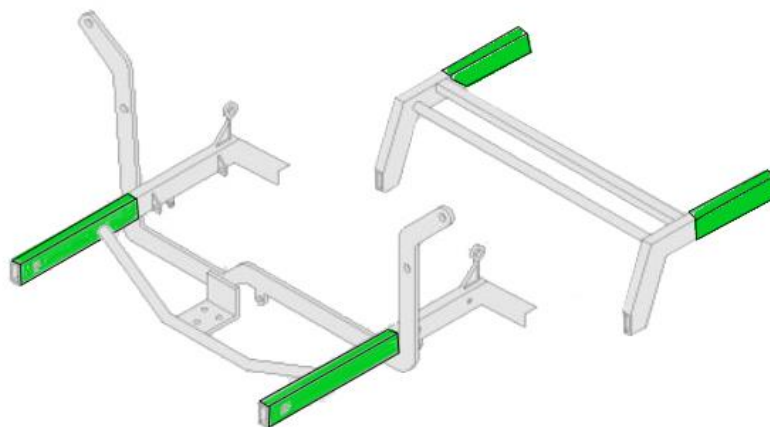
VIN Plates – All cars & frames running as Legend Cars must be manufactured and produced by USLCI. All cars must carry the VIN assigned to that frame. This plate shall not be tampered with in any way.

VIN number plates **MUST NOT** be removed or transferred to another chassis under any circumstances. Any participant found to be in breach of this rule will incur a lifetime ban from Legend Car racing in addition to heavy fines. No exceptions shall be made.

Metal Fatigue – It is highly recommended to have the frame checked periodically by an expert for metal fatigue. Cracked or broken frames are strictly prohibited from any LCA/INEX sanctioned events.

Front and Rear Clip repair zones.

Only the areas colored green may be repaired or replaced.
Anything beyond these zones requires a new clip.



Frame Replacement - If the chassis should become severely damaged, replacement frames must be purchased through LCA.

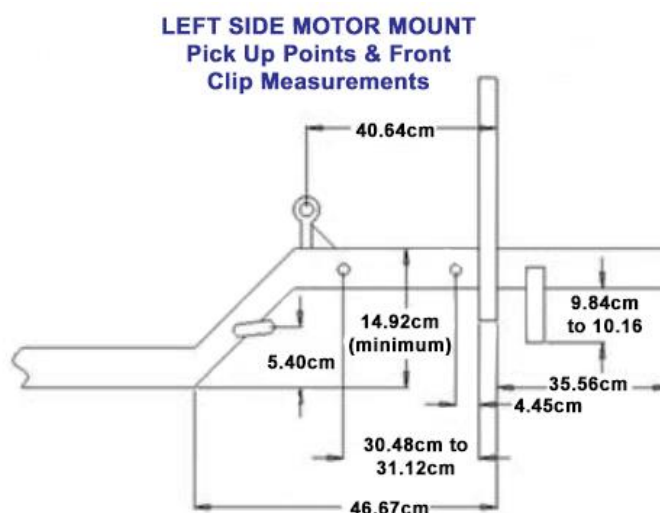
Frame Repair - Only front or rear clips may be replaced with the exact material that it is replacing, and all pick-up points must remain in the Stock locations as delivered new from USLCI. The clips must be purchased through LCA. The driver's compartment of the frame (roll bars, cross braces, etc.) may not be repaired or replaced if damaged (see *Frame Replacement* above). The INEX Technical Inspector reserves the right to disallow a repaired car from Competition that is not properly repaired.

Front and rear clips may be replaced with approved front and rear clips supplied and stamped by INEX or LCA. Repairs to front and rear clips are permitted as per the image below.

The main roll cage and drivers compartment is **NOT PERMITTED** to be replaced or repaired if damaged beyond the presiding technical officer's satisfaction.

LCA reserves the right to make the final decision with regard to the integrity of the roll cage/chassis.

Once a frame is repaired from its original form, LCA, INEX, USLCI and the INEX Technical Inspector assume no liability for any injuries that may occur because of this repair regardless of the approval given to compete with that repaired frame.



31. FUEL

The only fuel allowed is commercially available petroleum-based fuel available from multi outlet services stations, maximum octane99.

NO ethanol blend "E85", methanol, Elf or other brands of racing fuels are permitted.

NO additives are permitted. **NONE**.

LCA reserves the right to specify and/or supply at cost a designated fuel for specific events.

32. FUEL CELL

All fuel cells are to be as supplied by LCA, be stamped by INEX or LCA and must remain Stock and in the stock location. Metal fuel cells must be bolted through bolt holes in steel can.

Foam is **MANDATORY** in all fuel cells. Cars are not permitted on the track without foam in the fuel cell.

Red plastic fuel cells with the cap diameter of 5 5/8" (14.2cm) or larger are **NOT** permitted.

Fuel Cap – Check the cap on your fuel cell for tightness before going on the track. The fuel cap should fit snugly into the cell. **The car number must be clearly marked on the fuel cap.**

Fuel Cooling Devices - Fuel cooling devices of any kind are **NOT** permitted on the car at any time.

33. FUEL FILTERS

Aftermarket fuel filters may be used. Plastic fuel filters are allowed.

NO glass filters are permitted.

The fuel filter is not permitted in the engine compartment and must be located in Stock location behind the driver's seat firewall.

34. FUEL LINES

Fuel lines are **NOT** permitted to run through the driver's compartment. Steel braided or Kevlar braided fuel line is mandatory. The fuel line **may not** be attached to or contact electrical wires.

Fuel lines may be run through a sleeve and cable tied to wiring for safety.

35. FUEL PUMP

Fuel pumps must be stock as per supplied by LCA and stamped by INEX or LCA.

Fuel pumps must be wired to the engines electrical system so that when the engine is turned off electrically, so does the electrical supply to the fuel pump. The fuel pump should cease to operate when the engine stops. Fuel pump shall be securely mounted to the roll cage in the stock location as supplied by LCA.

36. FUEL VALVES AND REGULATORS

Aftermarket fuel shut off valves and fuel regulators are permitted and must be marked for the OFF position.

37. GAUGES & SWITCHES

Analogue gauges that record or display the following information **ONLY** are permitted:

- Cylinder head temperature.
- RPM.
- Oil pressure.
- Oil temperature.
- Voltmeter gauge

The only digital gauges permitted are:

- gear selection indicators
- oil temperature gauge
- oil pressure gauge
- Voltmeter gauge

Direct reading oil temperature and pressure gauges must use steel braided lines.

All switches must be clearly identified and marked accordingly.

NO mobile phones are permitted in cars during race or practice runs.

Data logging/recording and/or GPS devices are **NOT** permitted.

If a car is found with a gauge or gauges that violates these rules at any time, the gauge(s) will be confiscated, and the car will be disqualified from the event(s) it was used in. If the gauge is found during practice, the gauge will be confiscated.

38. GEAR RATIOS

The only rear end gear ratios permitted are to be within the range of 2.50 to 4.30.

The gears must remain within the Stock dimensions, thickness, weight, location and configurations as supplied by LCA and stamped by INEX or LCA. "Tumbling", polishing, sanding, etc. of the ring and pinion gears is not permitted.

39. GENERAL APPEARANCE OF THE CAR

All competitors must present a neat, clean and Stock appearing car for Competition. Crash damaged cars must be repaired to the minimum technical standards and to the satisfaction of the presiding technical officer/scrutineer before returning to Competition.

LCA reserves the right to deny access of a car into competition if in the opinion of LCA or their representatives, that car does not meet the minimum acceptable standards as mentioned above, is deemed too damaged or untidy, or the car has inappropriate sponsorship, advertisement, paint scheme and/or lettering on a car not in "good taste," or in the spirit of keeping this a family sport.

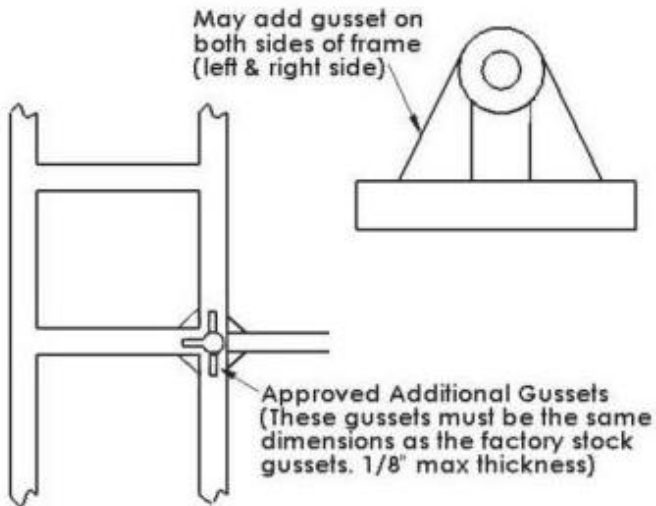
40. GRILL

The steel grill must remain within the Stock dimensions, height, steel thickness, location, and configurations and may not be reinforced in any way. The grill used must be the same model of the hood, grill shell & front fenders that are used. Grill brackets (to the frame) must remain Stock. Altering the brackets to raise or lower the grill is not permitted. Only wire screens are permitted to enclose the grill area (i.e. for dirt tracks to keep out mud and dirt). Duct tape or any other type of material other than a wire screen is not permitted to enclose the grill area. The radiator can be "taped off". The grill cannot be "taped off".

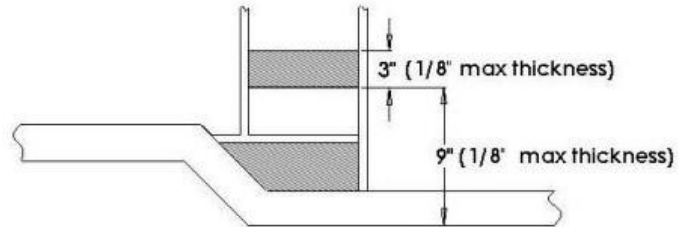
41. GUSSETS

Strengthening gussets may only be added in the locations as described in the Diagrams below.
No Stock gussets may be removed.

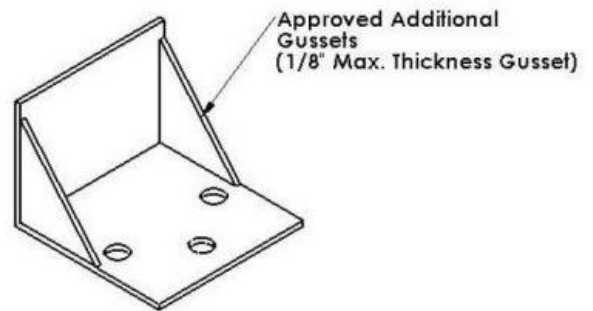
Upper Link & Rear Trailing Arm Pickup Points



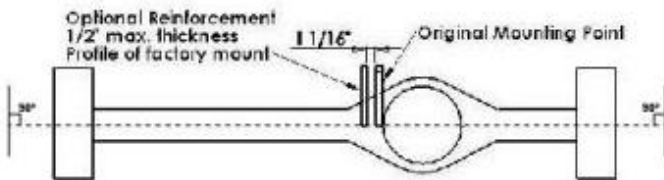
Front Clip Support



Steering Rack Plate



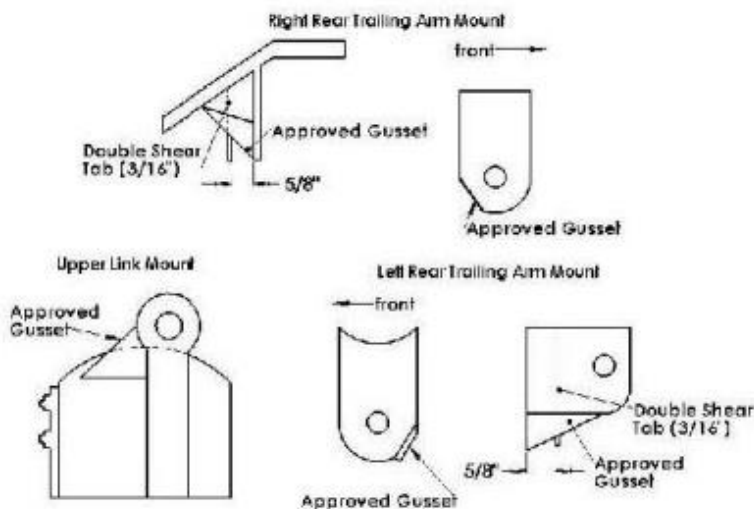
Rear End Housing Upper Link & Rear Trailing Arm Pickup Point



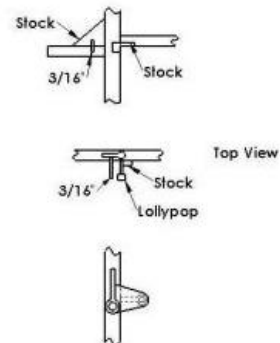
(This reinforcement will secure the opposite side of the rod end of the upper link)

Back View of Housing

Approved Rear Gussets (Max. 3/16" thick)



Upper Link Double Shear Mount (Chassis Side)



42. HEIM JOINT ENDS

Only magnetic steel Heims of similar dimensions as delivered new by LCA are permitted.

Aluminium heims are **NOT PERMITTED**.

NOTE: The Stock Heim joints are designed to bend/break and absorb energy under impact. Heim joints may be upgraded, however under impact, the upgraded Heim joints may not bend or break as quickly thereby transferring the impact energy to the driver and an injury may result (i.e. broken wrist from the transfer of energy).

43. HOOD

Hood louvers are permitted a maximum 3/8" (9.53 mm) in height. Louvers or holes in the hood may not exceed 54 Square inches (9" x 6") (229 mm x 152 mm). Replacing the Dzus fasteners located on the rear of the hood with hood pins is permitted. Raising the rear of the hood on the rear pins a **MAXIMUM** of 1" is permitted. While the car is on the track, the hood must be secure and may not move up or down on the pins. Hood louvers may not direct air to the carburetors or air filters. Air ducts may be used, mounted underneath inside the hood. The duct is not permitted to direct airflow onto the carburetors or air filters.

Hood Lengths - The minimum length permitted for 1934 Chevy and Ford hoods is 26" (660 mm) (measured down the middle). The minimum length permitted for 1937 Chevy and Ford hoods is 32" (813 mm) (measured down the middle). When using the FZ09/MT09 engine, the air filter may stick through the hood.

44. IGNITION SYSTEM

The complete ignition/engine control system must be the original OEM parts for the approved Yamaha motors. In-line fuses are permitted, but **NO** fuse blocks. Ignition pick up coil wires must run directly to the ignition box and may not be taped, or tie wrapped to other wires or fuel lines. No open wires or unused connectors allowed within reach of the driver.

Electronic throttle/traction controls are not permitted.

Ignition Control Box - The stock FJ1200/XJR1200/XJR1250 ignition control box that was the OEM supplied unit (black box), or the red ignition box (marked INEX-Approved) are the only boxes permitted to be used with the 1200 + 1250 engine and they **MAY NOT** be altered or relocated in any way. Only one ignition box is permitted on a car (multiple boxes are illegal). The original Stock FJ1200/XJR1200/XJR1250 rev limiting system must be in proper working condition and may not exceed 10,500 rpm.

The FZ09/MT09 must use the ignition control box (ECU) designed for use with that engine as supplied by LCA.

Swapping of the Ignition Control Box - INEX/LCA reserves the right to substitute an ignition/engine control system to any competitor at random. The INEX provided ignition/engine control system will be exchanged for a competitors system and must be used for that Event. INEX is not responsible for any engine failure due to a provided ignition/engine control system usage. Failure to comply will lead to expulsion from that event and possible fines and/or suspension.

Blue box ignition boxes or any other type of device that allows ignition timing to be altered are **NOT** permitted.

Coils, Coil Leads & Spark Plugs - These components must be the Stock OEM supplied parts to suit the engine used, stock grey INEX coil wires or any 8mm aftermarket coil wire (EG Bosch, Eagle etc.) or red Dynatek coils marked with the INEX logo as supplied by LCA and stamped by INEX or LCA. The FZ09/MT09 components must remain stock. The spark plugs may be aftermarket type with the same thread size. The stock coil mount must not be altered in any way and may only be replaced with an INEX or LCA approved coil mount. Resistors must remain in spark plug wire ends.

Coil Mount - The stock coil mount must not be altered in any way and may only be replaced with an LCA approved coil mount.

Ignition Rotors – The ignition rotor must be the Stock OEM supplied part, or the rotor delivered with the “INEX approved” red ignition control box. The FZ09/MT09 rotor must remain Stock. No Aftermarket electronic ignition advancers are permitted.

Ignition Plates - The ignition plate must be the Stock OEM supplied part or the plate delivered with the “INEX approved” red ignition control box. It may be slotted to advance the ignition timing of the engine. The ignition pickup cover is not mandatory. The FZ09/MT09 ignition plate must remain Stock.

45. JAM NUTS

A minimum of one jam nut is required to be used with all radius rods and ball joints. One or two (minimum 1/4” maximum 7/16”) jam nuts are permitted on the upper and lower ball joints.

46. KILL SWITCH

Kill switches are mandatory. Approved parts and placement are shown below. No modifications or alternate placement is permitted.

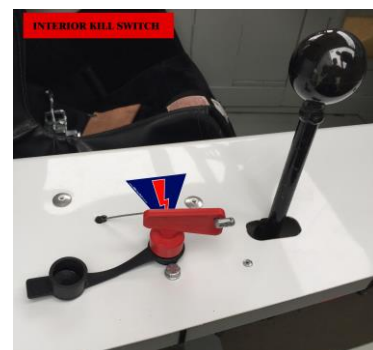
1200/1250 - RRS FIA Battery Master Cut-Out Switch:

The RRS FIA Battery Master Cut-out Switch will ensure complete electrical shutdown on a running competition car when operated. It also prevents damage to the alternator diode and prevents engine run on when operated. Suitable for 12V use and should be used with 10mm diameter battery terminals. Supplied with resistor.

STANDARD



HEAVY DUTY



Pull Cable

Pull cables suitable for remote operation of mechanical battery cut-off switches. The cable is supplied with an inner cable and outer sleeve with a threaded section to allow it to be mounted to a panel or bulkhead. This cable is to be mounted to the left-hand front side panel as per picture below and clearly marked with an FIA approved electrical decal.



FZ09/MT09

The external Pull cable is mandatory and needs to be attached to existing Kill Switch as per below images.



47. LOWER CONTROL ARMS

The lower control arms must remain Stock, within the Stock dimensions as supplied by LCA (12.5" x 16.25"), steel thickness, location and configurations and must be stamped by INEX or LCA. Each measurement will have a tolerance of +/-1/8" (0.125") (3 mm).

48. MANDATORY DECALS

All registered Legend Cars are to display LCA stickers (provided at Registration) on left and right rear panels as per image below. No changes are permitted to this decal positioning.



The current Series Sponsor visor sticker is to be affixed to the windscreen at all events. New stickers will be provided at Registration and replacement stickers can be obtained from LCA in the event of damage. The sticker must remain 100mm in height but may be trimmed at the outer corners to fit windscreen shape. Scrutineers have the right to deny a car entry to an event if the visor sticker is not affixed or affixed in a manner deemed acceptable.



LCA reserves the right to insist cars display a sponsor, association or incorporation logo at selected events. Failure to comply may result in expulsion from the event.

Failure to comply will result in R.o.F penalties for **ALL RACES** including Feature and no points allocated for that event.

49. MIRRORS

Mirrors are not permitted.

50. MUFFLERS

INEX-approved USLCI Stock S&S, or Borla mufflers are to be supplied by LCA and stamped by INEX or LCA and are **MANDATORY** for use with the Yamaha 1200 + 1250. The ProFab long exhaust system and quiet muffler as supplied by LCA is **MANDATORY** for use with Yamaha FZ09/MT09.

The muffler must remain Stock and may not be modified in any way (turning top away from car is permitted) except to add extra external tabs or mounts to assist in retention of the muffler. Mufflers may not be welded to the pipes. The internal components of the header and muffler may not be altered in any way.

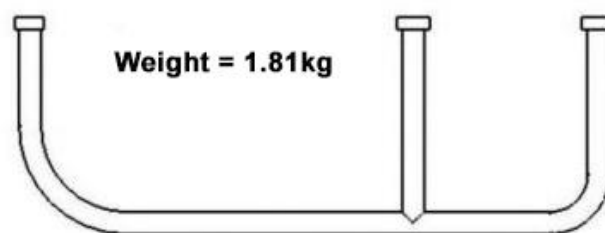
Exhaust must be mounted and fixed to the factory mounts (pushed tight together).

Generally, a chain of 3/16 thickness links fixed around the muffler outlet with the use of a hose clamp and the chain then bolted to the chassis gusset located nearby is sufficient. Other methods of retaining the muffler in place may be approved by the presiding technical official of LCA.

51. NERF BARS

The Nerf bars must remain within the Stock dimensions, steel thickness, location and configurations as delivered by LCA and stamped by INEX or LCA and may not be reinforced. Nerf bars must not be used to hold ballast. No other nerf bars are permitted.

LEFT & RIGHT NERF BARS



52. OIL CATCH CANS

An oil catch can (max 17oz (500ml) capacity) may be used. It must be securely fastened and remain within the engine compartment. It may only be routed to by a hose from the Stock crankcase breather opening or the oil fill cap.

53. 1200/1250 OIL COOLERS AND LINES

Only "Air" Oil coolers permitted (no dry ice or other type systems permitted). Oil coolers must be cooled by the flow of air only. All oil cooling systems (including lines) must be mounted in the engine compartment, forward of the front fire wall. Oil coolers **MUST NOT** be mounted below the bottom of the front bumper. More than one oil cooler is permitted. Oil cooler fans are permitted on oil coolers. Push-lock oil line fittings are permitted. Aftermarket oil coolers are permitted.

A shield securely fixed may be used to prevent dirt blocking the cooler, any such shield shall not exceed the front cooling core surface area of the cooler by more than 20%.

Oil Cooler/Radiator Scoops - Oil cooler/Radiator air scoops (with a maximum wall thickness of 1/8" (3.2 mm) sheet metal) must fit completely between frame rails and may not extend below or attach to the front bumper.

1200/1250 Overhead Oiling Systems - Aftermarket overhead oiling systems for the camshafts are permitted.

A shield securely fixed may be used to prevent dirt blocking the cooler, any such shield shall not exceed the front cooling core surface area of the cooler by more than 20%.

Oil Crankcase Breather – The only locations that the Yamaha 1200 + 1250 crankcase may have a breather are under the carburetors at the Stock outlet or in the oil fill cap. The FZ09/MT09 crankcase breather must remain in its Stock location. The crankcase breather may not be evacuated through the exhaust pipe (header). There may not be additional breather holes for the crankcase. Breathers and hoses must remain within the engine compartment. Baffles used in the hoses are permitted.

54. OIL FILTER - REMOTE

Remote oil filters are permitted to be used on Yamaha 1200 + 1250 engines mounted in the engine compartment only. Inside or outside mounting of the remote oil filter to the frame rail is permitted. The remote oil filter must be located where it cannot be easily damaged in the event of an accident. Remote oil filter may not be mounted below the bottom of the front bumper. Filter must have a hose clamp around it, safely wired to the mount (to prevent it from backing off).

Remote oil filter is **NOT** permitted on the Yamaha FZ09/MT09 engine.

55. PICKUP POINTS AND SPACERS

Absolutely **NO** modifications of the frame pickup points, rear end pickup points or spindle points are permitted. A maximum 3/4" (1.9cm) wide spacer may be used on all 1/2" (1.3cm) suspension bolts.

56. RACK AND PINION STEERING

Only the rack and pinion steering box as supplied by LCA and stamped by INEX or LCA is permitted. **NO** other steering box is permitted.

The rack and pinion mounting plate measures 3 7/8" to 4" from the bottom of the frame rail to the top of the mounting plate. A 4"x 4" x 1/8" plate will be allowed to be bolted (not welded) under the steering rack mounting plate. This will be a 4"x 4" x 1/8" plate with three holes using the steering rack studs to secure it. A 1/4" thick steering rack plate is permitted (as delivered by LCA).

57. RADIOS

The only type of communication allowed is a Raceceiver (that is controlled by the Race Director/Chief Steward). Cell phones or any other source of communication that connects the Driver to any outside source is not permitted. (E.I.R.I.). **Two-way radios/communication is NOT permitted.**

58. RADIUS RODS AND PANHARD BAR

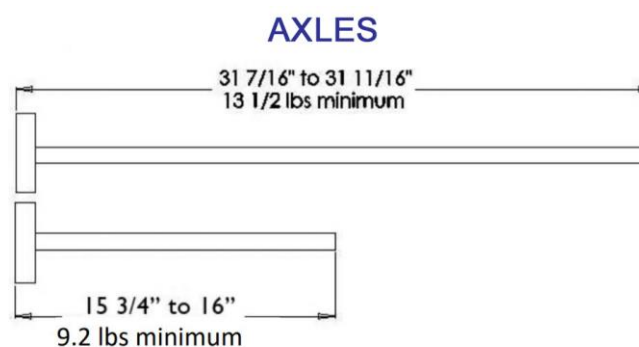
The aluminium Radius Rods and Panhard bar must remain within the Stock dimensions (Radius Rods 6"-6.5", 11.0" or 12.0" in length / Panhard bar 23.5" in length), thickness, location, and configurations, as supplied by LCA and stamped by INEX or LCA. Each length measurement will have a tolerance of +/-1/8" (0.125") (3.2 mm). The 11" (279.4mm) and 12" (304.8mm) rods may be interchanged anywhere on the car as long as the car still meets all specifications contained in this rule book.

Radius Rods and Panhard bars **MUST NOT** be reinforced in any way.

Steel Radius Rods or Panhard bars are **NOT** permitted.

59. REAR AXLES

The long and short rear axles must remain Stock, within the Stock dimensions, steel thickness, location and configurations and may not be reinforced. The only axles permitted are the type that are supplied by LCA and stamped by INEX or LCA and must not be lightened or strengthened in any way. All one-piece axles must be stamped by INEX or LCA.



60. REAR ENDS

Only 10 bolt pattern/wide flange (5/8") Toyota, locked steel rear ends are permitted.

All rear end components, to include the housing and pickup points must meet the specifications of the Stock component as supplied by LCA. Strengthening of upper rear diff mount is acceptable with materials remaining OEM. Any repair is to be approved and recorded in the Logbook by LCA before next race meeting. The rear end must be locked (all spider gears welded or steel spool of a minimum 6.0 lbs/2.72kg) as supplied by LCA and stamped by INEX or LCA.

NO limited slip differentials, aftermarket differentials, quick change rear ends, floaters, homemade or otherwise are permitted.

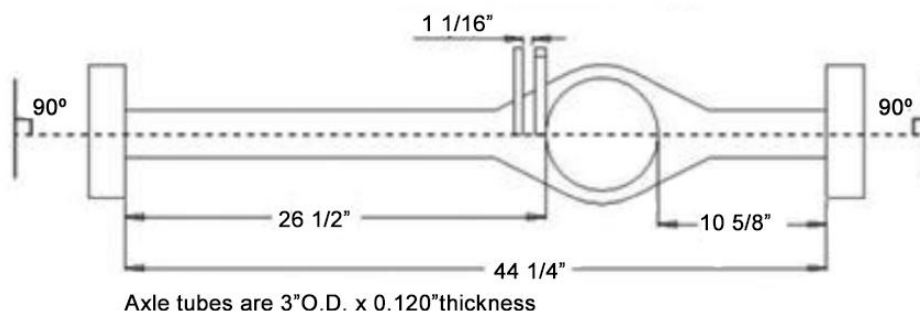
NO "tumbling", polishing, sanding, etc. of the ring and pinion or any internal parts of the rear end is permitted. Coatings/finishes must remain stock (as delivered by LCA).

Only OEM bearings are permitted.

NO hemispheric, ceramic coated or similar type bearings are permitted.

NO spacers are permitted between the backing plate and bearings. Axle tube material must be 3" O.D and 0.120" wall thickness. "Double shear" rear end housing is available through LCA and may be used. Axle bearing flanges are square to the centre line of the rear end housing.

REAR END HOUSING



61. REAR OFFSET

There must be a minimum of 6" between the inside edge of the rear brake drum and the outside rear frame rails (directly below the centreline of the rear end housing) on both sides.

NO wheel, brake drum or axle spacers are permitted.

62. RIDE HEIGHT

A car must maintain 3 1/2 inches (89mm) (minimum) height, between the bottom of the frame rails (not the weld) and the surface. **NOTE:** This measurement is to be checked without the Driver in the car, as Raced, without lifting of the car in any manner. The inspection "location or spot" used to check ride height is determined by the INEX/LCA Technical Inspector/Scrutineer at that time.

63. RUNNING BOARDS

The running boards must remain within the Stock dimensions, steel thickness, location and configurations as supplied by LCA and approved by INEX or LCA.

NO reinforcement permitted.

64. SAFETY GRILL

An LCA certified Safety Grill is mandatory for all Speedway Legend Car competition in Australia. LCA certified grills are identifiable by the certification stamp.

65. SHEET METAL

The minimum thickness of sheet metal is 0.036" (0.9 mm). The rear deck sheet metal (including the package tray behind the Driver) may not be removed or altered in any way unless a fuel cell access hole is used. The fuel cell access hole must always be covered with a sheet metal plate and secured while the car is on the track.

Dash: The dash is mandatory. If dash is replaced it must weigh a minimum of 0.50 lbs (0.272 kg) and cover both dash bars. Plastic or Lexan dash is permitted. A carbon fibre dash is **NOT** permitted. If extended switches are needed (for smaller drivers), they will need to be located on the driveshaft tunnel (by the shifter).

66. SHIFTER, LINKAGE & KNOB

Shifter linkage must be Stock. Maximum height of the shifter handle is 300mm as measured from the top of the driveshaft tunnel to the top of the shifter handle. Gear knob may be stock or driver choice but must not exceed the 300mm height requirement above. Gear knob choice may not be offensive in any manner.

67. SHOCK ABSORBERS

Legend Cars must use the INEX or LCA stamped Bilstein shocks or LCA approved AFCO Legends shocks with a compression/rebound rate of 4/5 valve **ONLY** (with the AFCO / INEX tamper evident sticker sealing the cap) as supplied by LCA. Shock numbers must be made visible at the request of an INEX or LCA Technical Inspector. Bilstein and AFCO shocks **ARE PERMITTED** to be mixed e.g. you can run 3 Bilstein shocks and 1 AFCO shock on the same car.

Shock absorbers may be mounted either way up. The upper part of the shock may be mounted inside or outside of the frame. Shock bumpers are permitted (max ¼" (6.35mm) tall, "un-compressed"). Shock covers are permitted but must be removed if directed by Scrutineers.

Absolutely no tampering or alteration of external or internal components, fluids or gases permitted. Competing with a modified or altered shock(s) will result in suspension of the driver. Removal of the AFCO void sticker(s) will result in disqualification and/or suspension.

AFCO shocks are permitted to be rebuilt **ONLY** by the nominated INEX approved company:

AFCO Shock Doctor

102 Kensington Dr, Munruben, QLD
4125

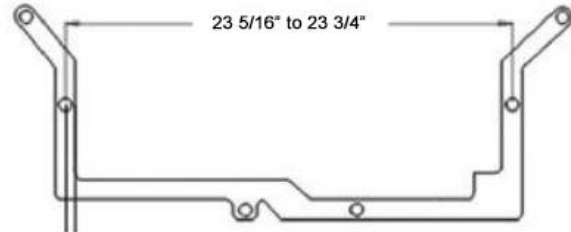
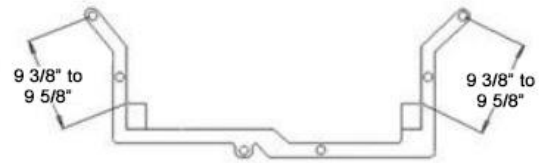
shockdoc@bigpond.net.au

(07) 3297 1284

0407 154 680

Warranty of AFCO shocks will only be available to customers who purchase shocks in Australia/from LCA.

SHOCK TOWER



68. SPINDLES

The Aluminium spindles (and the spindle pick-up points) must remain Stock, within the Stock dimensions, thickness, location, and configurations as supplied by LCA and stamped by INEX or LCA.

Repairing a broken or cracked aluminium spindle is **NOT** permitted.

Only alloy spindles as issued by LCA are permitted, **NO** steel versions are allowed.

69. SPRINGS

All Legend Cars must use a 10" (25.4cm) or 8" (20.32cm) spring. Any spring weight combination and aftermarket springs of Stock design are permitted (stock springs are the same outside diameter from top to bottom). Barrel springs, progressive springs or any springs that are not stock design are **NOT** permitted. Only one spring per shock is permitted.

Spring rubbers are **NOT** permitted.

70. SPROCKET ADAPTORS

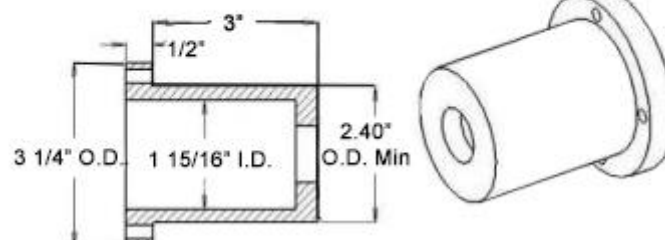
The sprocket adapters must remain Stock, within the Stock dimensions, steel thickness, location, and configuration as supplied by LCA and stamped by INEX or LCA. Set screws for the sprocket adapter nut are permitted. **NO** lightening or modifying the sprocket adapter in any manner is permitted.

1200/1250 engine = steel adapter as supplied by LCA (diagram below).

FZ09/MT09 engine = aluminium adapter as supplied by LCA.

SPROCKET ADAPTER

2 1/4 lbs Min. Weight



71. STEERING COLUMN

The steel steering shaft or steel steering column bracket may be modified for Driver comfort or safety by altering the length of the shaft or by altering the steering column bracket that connects to the dash bracket. Dash bracket must remain steel. Aluminium "bolt on" steering brackets or aluminium Heim joint ends are **NOT** permitted. Bearings are **NOT** permitted to be used in mounting the steering shaft. Stock-style bushings and steel rod-ends must be used. Intentional lightening of any component is **NOT** permitted. A hose clamp or shaft collar is **MANDATORY** on the steering column in the engine compartment directly against the firewall. There can be no more than a 1/4" (6mm) gap between the clamp (collar) and the bushing. Modification of the driver's compartment roll cage is **NOT** permitted. Steering quickeners are **NOT** permitted.

Upgraded steering universals are permitted.

Steering Shaft -Tubing used for steering shafts must be Stock as supplied by LCA and may not be reinforced in any manner. Upgraded steering shaft joints are acceptable.

72. STEERING WHEELS

Larger or smaller steel or aluminium steering wheels are permitted. INEX/LCA approved quick release steering hubs are **mandatory**.

73. TRACTION CONTROL DEVICES

NO electronic or computerized wheel spin/traction control device(s) is permitted to be installed in the car, whether operational or not. A car with these device(s) will subject the Driver to an INEX suspension.

74. TYRES

ZESTINO 22.5/8.0-13GT Legend Tyre, stamped and supplied by LCA, are permitted. Tyres must not record a reading of less than 45 on a durometer.

Any tyre that does not meet the minimum durometer reading of 45 will be confiscated by the Scrutineer for further testing and may be destroyed at the discretion of the presiding technical officer.

It is the responsibility of car owners and drivers to ensure their tyres comply with the measuring device used by scrutineers.

Tyres used on the car are **NOT** to be recapped, repaired, patched, siped, ground, machined down, soaked or softened.

NO substance that softens or alters the compound shall be used on the tyre footprint area.

Tyre shine or appearance enhancer may be used on the **side walls only**.

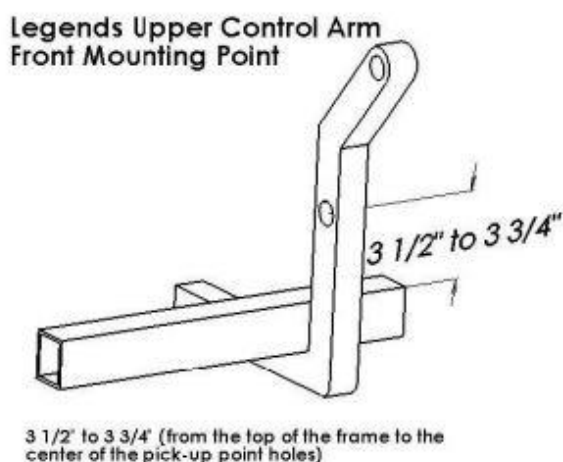
Cars must **NOT** have any tyre other than the prescribed tyres at any time during the course of a race meeting including transport through the pit area.

Heavy penalties will be imposed by LCA for any driver in breach of these rules relating to tyres. Tyres to be run stock. **NO** grooving allowed.

At the direction of LCA a different tyre may be used for testing purposes, but no points or prizes/prize money will be allocated to that car.

75. UPPER CONTROL ARMS

The upper control arms must remain Stock, within the Stock dimensions (4.25" +/-1/8" (3.18mm) in length), steel thickness, location, and configurations as supplied by LCA and stamped with INEX or LCA and may not be altered in any manner, as shown below.



76. WEIGHT

1200/1250:

The minimum weight is 1322lbs (600kg) with driver and in full racing condition after practice or race.

FZ09/MT09:

The minimum weight is 1257 lbs (570kg) with driver and in full racing condition after practice or race.

The Scrutineer will designate the scales to be used for any post-race checks and they will be deemed to be Championship scales.

Competitors are **NOT** permitted to add fuel, fluids, or intentionally removed components after an Event to meet minimum weight requirements.

It is the driver's responsibility to ensure the car meets the minimum weight requirement of the measuring device of the LCA officials.

Weight Reduction Measures – Lightening or modifying Stock components by shaving, milling, drilling or any other method is **NOT** permitted. Components must remain the same material unless specified in this rule book. Any component on a Legend Car that is used (other than what is specified) as a weight saving method is **NOT** permitted and considered **ILLEGAL**. All non-stock aluminium, titanium, stainless steel, etc. components are considered **ILLEGAL** and will be confiscated.

NO weight reduction of the chassis or frame is permitted.

Weight may be adjusted throughout the year between the different engines.

77. WHEELBASE

All cars must compete with 72 ¾" (184.78cm) to 73 ¼" (186.05cm) wheelbase on either side.

Measuring wheelbase – The measurement will be taken with the front and rear tyres on one side in line with each other. To determine this, the measuring tool or tape measure must touch three points. These points are the front of the rear tyre sidewall and the front and rear of the front tyre sidewall. The measurement is then taken from the front edge of the front wheel to the front edge of the rear wheel. The process is the same when measuring wheelbase on the opposite side.

78. WHEELS

Any type of automotive wheel that has a 13" (33cm) diameter, a 7" (17.8cm) width and the offset of 3" (7.6cm) to 3 ¼" (8.25cm) from the back rim edge to the back of the wheel centre is permitted.

All wheels must be magnetic steel, **NO** alloy wheels permitted.

NO wheel spacers are permitted.

All wheel weights must be secured with duct tape. Bleeder or relief valves are **NOT** permitted.

LCA approved (and INEX marked) bead lock wheels **MUST** be used on the right rear and are permitted to be used on the other three corners of the car.

79. WIDTH

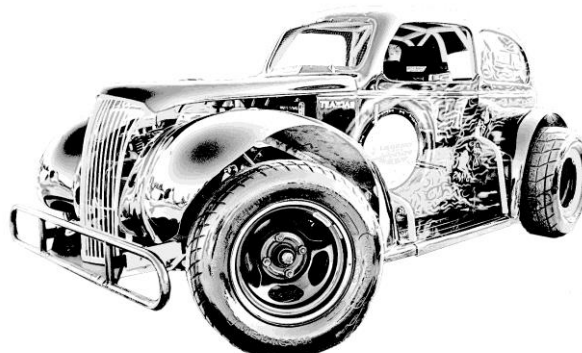
The total overall width of the car (front and rear) may not exceed 61 inches (154.94 cm) maximum with LCA approved tyres. The car must be able to roll freely through a 61" (154.94 cm) wide opening as raced. Spacers on the rear wheels, axle, drums etc. are NOT permitted.

80. WINDSHIELD MESH

Windshield mesh must remain stock as supplied by LCA.

Sun visor maximum 110mm, minimum 100mm is to be used at the top of the windscreen opening, must be of flexible plastic type material and be securely fixed. LCA reserve the right to advertise series or other sponsors in this location as per **48. Mandatory Decals**. Failure to comply will result in R.o.F penalties for **ALL RACES** including Feature and no points allocated for that event.

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## **PART FOUR**

**If this rule book does not specifically say that you can change, modify, add or remove something then you must consider that the change, modification, addition or removal of that item is ILLEGAL.**

### **FZ09 & MT09 ENGINE SPECIFICATION RULES**

The FZ09 and MT09 engines **MUST** be purchased from LCA.

The FZ09 and MT09 are sealed engines. Tampering, removing, or altering the engine seals will result in an immediate disqualification, engine confiscation, loss of highest 5 Race (including state or national Title) finishing positions, and a period of 12 months of probation. Additionally, the Driver, and/or car owner are subject to further penalties.

All components of the FZ09 and MT09 engines must remain Stock, in Stock location and configuration. The FZ09 and MT09 engine must be installed exactly as the engine is installed at USLCI and include all factory Stock parts and pieces.

Any work carried out must be to standard specifications and within Stock dimensions, location and configuration as delivered new from LCA and use Stock Yamaha parts **ONLY**. An itemised receipt with all parts and a detailed description of all works completed **MUST** be sent to Tony Ward if using an external mechanic.

Competing with an engine that does not internally conform to the USLCI Engine Shop specifications will result in an immediate disqualification, engine confiscation, loss of highest 5 Race (including state or national Title) finishing positions, and a period of 12 months of probation. Additionally, the Driver, and/or car owner are subject to further penalties.

### **1300, 1250 & 1219 ENGINE SPECIFICATION RULES**

The only modifications allowed to the factory sealed XJ engines are:

- A. Changing carburetor jets
- B. All Stock carburetor adjusters may be used
- C. Adjusting the valve shims
- D. Installing an Aftermarket clutch and spring of original design (no aluminium clutch plates).
- E. Upper head oilers, heavy-duty valve springs, "pinned" camshafts and steel sleeves are permitted.

### **FJ & XJ 1200 ENGINE SPECIFICATION RULES**

All engines must remain a factory-Stock Yamaha FJ1200/XJR1200 as delivered new. Only those changes as described in the rulebook are permitted to be made to the FJ1200 and XJR1200.

See the INEX rule book page 78 for technical engine specifications at:

<https://www.inexseries.com/documents/2024-inex-rulebook-digital-version.pdf>

## **1200 CAMSHAFTS**

Camshaft modifications are **NOT** permitted in XJR1250/XJR1300 engines.

**Cam Shaft** - The camshaft must remain Stock FJ1200/XJR1200 as delivered by Yamaha.

**Cam Chain** - The cam chain must remain Stock FJ1200/XJR1200 as delivered by Yamaha.

**Cam Chain Adjuster** - Aftermarket manual cam chain adjuster is permitted to be used.

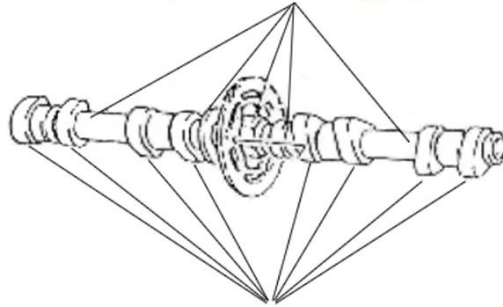
**Cam Timing** – cam timing may be adjusted accordingly to meet the cranking compression requirements.

Stakeable link can be used.

Camshafts may be pinned.

### **STOCK YAMAHA CAMSHAFT**

**STRESS RELIEVING THE CAMSHAFTS**  
INEX has approved polishing the shaft of the cam  
in these areas (for stress relieving purposes only).



**You may not alter the cam lobe in any way.**  
**(The sides of the cam lobe must remain untouched, with casting marks on them).**

## **ALL AIR-COOLED ENGINE TYPES - SPECIFICATION RULES**

### **CLUTCH**

The clutch (plates and springs) may be replaced with aftermarket types of the same design.

**NO** aluminium clutch plates allowed. Aluminium Clutch baskets permitted. Slipper clutch is **NOT** permitted. Heavy Duty Clutch is permitted.

The basket, primary drive gear, shims and springs must weigh a minimum of 5.5 lbs (2.49 kg).

### **CYLINDER HEADS**

All engines must remain in unaltered “as cast” condition and retain casting marks on inlet and exhaust ports, except for machining the gasket surface (no angle milling allowed).

All engines are permitted to have valve seat inserts reworked or replaced with OEM or aftermarket seats of original dimensions.

The stock I.D. at the bottom 1/16" of the intake port is 0.990" and 0.830" for the exhaust port. **NO** porting or port alterations of the intake or the exhaust ports are permitted. "O-ringing" of the head gasket seal area, head or block is **NOT** permitted.

**Combustion Chamber** - Relief cuts for cc balancing must **NOT EXCEED** the bore size.

**Repairing a Cylinder Head** – Repair welding on heads is permitted but must remain stock configuration.

## **CRANK SHAFT**

The crankshaft must remain Stock OEM to suit the Yamaha engine used. Modifying the throw balances or weight of the crank is **NOT** permitted. Polishing the crankshaft is **NOT** permitted.

Minimum crankshaft weight is 27 lbs (12.2 kg).

## **PISTONS & RINGS**

### **1200:**

The cylinder bore may not exceed 77.57 mm or 3.054" (maximum .022" overbore from Stock). Pistons may be oversize type up to 0.022" from Stock. Pistons must remain complete with **NO** alterations.

Cutting the dome on 1200 pistons **ONLY** is allowed in order to achieve 12.5:1 compression ratio.

### **1250:**

Only two (2) oversizes for 1250/1300 Sealed Engine are permitted: + 4Thou and +8Thou.

### **1200/1250:**

**Cylinder Sleeves** – Worn cylinder sleeves may be replaced with aftermarket sleeves.

**Pistons** - Yamaha, Wiseco or Aftermarket type pistons are permitted. The ring lances and lower part (skirts) of the piston **may not** be altered from the original in any manner. Gas-porting the pistons are **NOT** permitted.

**Rings** – Yamaha, Wiseco or Aftermarket type rings are permitted. Rings are not to be altered in any manner.

## **CONRODS**

The conrod must remain Stock OEM to suit the Yamaha engine used. Rod bolts may be changed to any aftermarket type. All dimensions must be Stock.

**NO** polishing of Conrods is permitted.

All Conrods must be as per original assignment, e.g. no 1250 rods in a 1200 engine.

## **STROKE**

The stroke must remain Stock.

## ALL ENGINE TYPES - SPECIFICATION RULES

### COMPRESSION RATIO

The compression ratio shall **NOT EXCEED**:

1200/1250 – 12.5:1 max.

FZ09/MT09 – 15.3:1 max

If the compression ratio is greater than that listed above, it will result in an immediate disqualification and confiscation of all the following parts including: the head, the valves, the head gasket, etc. as well as the pistons, and cylinder casings (including base gasket).

Upon rebuilding, only the following are permitted: machining the piston domes and gasket surfaces of the cylinder head and/or cylinder casing.

### CRANKING COMPRESSION

The cranking compression shall **NOT EXCEED**:

1200/1250: 185 psi max.

FZ09/MT09: 225 psi max.

The cranking compression must be at or below the relevant engines limit at any time (hot or cold) on at least three (3) cylinders with the ignition off, the throttle wide open, and with the carburetors on the engine. Ten to twelve revolutions of the engine will determine the total compression of the cylinder. Cam timing may be adjusted accordingly **FOR 1200 MOTORS ONLY** to meet the requirements of this rule by slotting the cam sprocket gear or substituting with an aftermarket cam gear sprocket.

INEX and LCA highly recommends that you have your engine set at a minimum of 10 psi below the relevant engines maximum cranking compression instead of right on the line to allow for slight variations between gauges. There is no tolerance for engines over the maximum psi. The gauge used by the Technical Inspector/Scrutineer is the “official gauge.”

## **PART FIVE**

### **VALVES**

Only Stock OEM valves to suit the Yamaha engine used are permitted.

The only alterations to valves are the angles on the valve seat, with a maximum back cut width of up to 0.200" from the edge of the valve is permitted. Only stock valve lifter and adjuster shims may be used.

**Valve Guides** - The valve guides must remain the same size, length, and location of the current Stock valve guides. The guides may not be altered in any manner. Brass aftermarket value guides of the same dimensions of Stock may be used.

**Valve Springs** – The valve springs may be replaced with aftermarket magnetic steel springs. Only Stock steel valve spring retainers and clips/keepers are permitted. No titanium or other lightweight alloys are permitted.

### **TRANSMISSION**

The transmission and associated transmission gears must remain Stock Yamaha to suit the engine used in the Stock configuration. They may not be modified or lightened in any way. The only alterations permitted being the installation of shims to assist with proper gear engagement. Standard and aftermarket shims may be utilised.

### **OIL PANS, PUMPS AND OILING SYSTEMS**

#### ***Oil Pan***

The stock oil pan may **NOT** be altered, modified, or include any additions to alter the path of oil.

#### ***Oil Pump***

The Stock oil pump may **NOT** be modified in any way, except for the installation of shims in the relief valve.

#### ***Oiling Systems***

Oil lines may be diverted to suit remote oil coolers, filters or camshaft oilers (commonly known as top oilers).

## **PART SIX**

### **REGISTRATIONS**

All race cars must be registered with LCA, have the Logbook available and carry an LCA approved registration sticker. Registration period is from 1 Jul to 31 Jun of the current year. Registration sticker must be fixed to the Roll Cage on front left-hand side parallel cross bar. Secondary VIN Plate (available through LCA) must also be affixed to the chassis on the middle right cross bar below gear shift.

All Logbooks will be completed in the name of the legal owner of the car. All owners must carry current Associate INEX Membership. Owner or driver may sign documents pertaining to the car. A letter of authorisation (please contact LCA) must be completed if the driver is permitted to charge parts or fees to the car owners account.

All cars and frames racing as Legend Cars must have been produced by USLCI and stamped by INEX or LCA.

Cars not registered with LCA and not having a registration sticker approved by LCA will not be eligible to compete in Australia.

Registration stickers must not be damaged, removed or transferred to another car or chassis. Replacement Registration stickers in the event of damage are available from LCA at no charge.

INEX membership is mandatory for all drivers and optional for Associates. INEX membership is valid from 01 July to 30 June each year. Australian INEX membership has to be approved by LCA.

***The onus falls to the driver to race a safe race car. Annual Registration and random scrutineering in no way guarantees the safety of the car. This applies to all safety equipment also.***

### **INEX MEMBERSHIP**

#### **INEX MEMBERSHIPS (Member and Associate):**

INEX membership is mandatory for all drivers. Associate membership is optional for family and crew etc.

All Competitors must have submitted an INEX membership application to be permitted to compete in any INEX Sanctioned Event. To receive points for competing, each Driver must have been issued and maintain a valid INEX membership and INEX member number. Without a membership a competitor may be denied participation in an INEX Sanctioned Event, unless they receive prior approval from INEX Headquarters when registering for the Event.

Application forms for an INEX membership can be obtained by emailing: [admin@legendcarsaustralia.com.au](mailto:admin@legendcarsaustralia.com.au). LCA is solely responsible for approving and processing such memberships. Membership application and release forms must be fully, signed by the applicant, and/or parent, or legal guardian (if applicable) and accompanied by the appropriate funds. The applicant will be considered a member of INEX upon the issuance of the INEX Membership number. **INEX reserves the right to deny membership to any applicant for any reason in INEX's sole and absolute discretion. Applicants will be advised in writing by the LCA if their application for INEX membership has been denied. All memberships are provisionally in effect once the application is submitted to LCA.** Memberships are issued on an annual basis beginning July 1 and expiring June 30 unless otherwise noted.



ALL INEX members and Associate Members are independent contractors and are neither agents, servants, nor employees of INEX. INEX members assume and take full responsibility for reporting and paying to the appropriate authorities all charges, premiums, and taxes, if any, due or payable on funds or benefits INEX members may receive because of their participation in INEX Sanctioned Event(s).

**Membership requirements for individuals under 18 years of age:**

If the applicant is under 18 years of age, a minor/ parent and/or guardian releases signed must be executed. A government issued birth certificate must also be submitted with the INEX membership application and all release forms.

**A MINOR applicant (age 17 and under) must also have a parent(s) or guardian(s) as an INEX Associate Member.** Any documentation provided that falsely represents name, age, date of birth, or birthplace, could result in immediate and indefinite suspension.

Any Driver under 18 years of age must receive approval to compete from the Track Promoter prior to the Event, to be eligible to compete at that Track. The Track Promoter's decision is FINAL. INEX has no authority in this matter, or any Track matters, regarding Driver's age.

**Membership Violations:**

Any INEX member(s) that loans or allows another individual(s) to attempt to obtain pit passes or other credentials for an Event(s) or other membership benefits by the use of membership number is subject to Penalty that may result in suspension of their membership. Any documentation provided that falsely represents name, age, date of birth, or birthplace, could result in immediate and indefinite suspension.

## **DRIVER DIVISIONAL STRUCTURES**

- 1) **PRO** (Expert Class): **Designed for experienced Drivers.** The Driver must have competed in some other form of professional motorsports or have competed in the Semi-Pro or Masters division to be eligible. This is the pinnacle class of Legend Cars racing and will be treated as such. Approval to compete in this class will be the decision of the LCA Director.
- 2) **MASTERS: Drivers 40 years old and over.** If a Driver reaches 40 years of age during the current season, that Driver will have the option to advance to Masters on their birthday. This move is not automatic, and a written request is required from the Driver to the LCA Director.
- 3) **SEMI-PRO: Drivers 16 years old and older (E.I.R.I.):** Designed for Drivers who are starting out in Legend Car racing. No prior experience is necessary.

**Divisional (Class) Change:**

Any Driver desiring to change division must first receive approval from the LCA Director in writing. Drivers requesting to move down in status must show proof that they are not properly classified. Points accumulated during the season will not be transferred if a driver changes their division during the season.

LCA reserves the right to place any Driver into a different division (up or down) at its own discretion at any time. Professional drivers entering Legend Car racing will be required to race in the pro division (E.I.R.I.). A Driver cannot change division once nominations are posted for the next race event or at the track the same day of a Race and cannot change points and/or classification prior to approval of the division change by the LCA Director.

## INEX POINTS

1. INEX points will be accrued as per calendared rounds. Events added after the finalisation of calendars will be listed as non-points rounds, unless approved by LCA. Information on each calendared round can be found on the website in 'further information' for each individual event.
2. Only Drivers receive points, not the car or team owners.
3. All rescheduled races will be on off days when allowable (with LCA approval). Rescheduled Events for the following day will be allowed without prior INEX or LCA approval.
4. A Driver must compete in a feature race or semi-feature race (B-main) to receive points. There will be no points awarded if a Driver does not advance past the initial qualifying Events. (E.I.R.I.).
5. Points are non-transferrable between divisions. The Division you declare at the beginning of the season on your INEX membership application and approved by the LCA Director is where you will accumulate points. If a Driver changes divisions mid-season with the approval of the LCA Director, the Driver will begin a new point accumulation for that division and not be eligible for awards/honours earned in the former division.
6. All calendared races (features) count for INEX points including Titles.
7. Any INEX points accrued when a driver competes overseas will not be counted towards any Australian Championships.
8. Interpretation of the rules as they apply to issuing points shall be at the discretion of LCA
9. B-Feature Race
  - a. Transfer spots to A-Feature receive zero (0) points until finish in A-Feature.
  - b. Non-transfer spots from the B-main begin at fifty (50) points and reduce by one (1) point per position.
  - c. Technical Disqualifications in a feature Event will receive zero (0) points.

### **10. POINTS SCHEDULE** **Points are awarded as follows:**

| <b>Finish</b>    | <b>Points</b> | <b>Finish</b>    | <b>Points</b> |
|------------------|---------------|------------------|---------------|
| 1 <sup>st</sup>  | 100           | 15 <sup>th</sup> | 68            |
| 2 <sup>nd</sup>  | 97            | 16 <sup>th</sup> | 67            |
| 3 <sup>rd</sup>  | 94            | 17 <sup>th</sup> | 66            |
| 4 <sup>th</sup>  | 91            | 18 <sup>th</sup> | 65            |
| 5 <sup>th</sup>  | 88            | 19 <sup>th</sup> | 64            |
| 6 <sup>th</sup>  | 86            | 20 <sup>th</sup> | 63            |
| 7 <sup>th</sup>  | 84            | 21 <sup>st</sup> | 62            |
| 8 <sup>th</sup>  | 82            | 22 <sup>nd</sup> | 61            |
| 9 <sup>th</sup>  | 80            | 23 <sup>rd</sup> | 60            |
| 10 <sup>th</sup> | 78            | 24 <sup>th</sup> | 59            |
| 11 <sup>th</sup> | 76            | 25 <sup>th</sup> | 58            |
| 12 <sup>th</sup> | 74            | 26 <sup>th</sup> | 57            |
| 13 <sup>th</sup> | 72            | 27 <sup>th</sup> | 56            |
| 14 <sup>th</sup> | 70            | 28 <sup>th</sup> | 55            |

11. It is prohibited that any INEX Member reserve a facility for private practice or testing on the site of any Championship (Titles) event one week prior to the event. Practice open to all INEX Members is acceptable. Any INEX Member that violates this policy will not be permitted to participate in the Championship (Title) event at that facility.

## NOMINATIONS

All nominations are to be completed online through the Legend Cars Australia website, [www.legendcarsaustralia.com.au](http://www.legendcarsaustralia.com.au). Unless noted in 'further details', in the individual event, all nominations will close at 8am on the Monday prior to the race date. The nomination system will remain open until midnight on that day and late nominations will be accepted until close. It is the drivers responsibility to read any and all information pertaining to an individual event. ALL entries received after 8am will be considered a Late Nomination. Late nominations will start rear of field and will be awarded finishing points only if heats are run, but no passing points. Failure to notify of a cancelled nomination will result in a \$50 penalty or Rear of Field start for the entire meeting, including Feature race for their next nominated meeting. Rolling starts will be mandatory. New drivers will start rear of field until officials deem them safe to go in the general draw.

Promoters will have the option to allow any nominated driver to start from any position at their discretion in conjunction with LCA officials.

Where tracks are running limited numbers or are capped, first preference will be offered to the top point scores in that Series/Championship. If the numbers are not filled by these position holders, then the next position holder numerically is to be given the option of nomination.

Nominations for the Development Series will be accepted when Development Series is running, however, if a Development driver elects to run in the Main division when Development Series is not running, they will be required to start Rear of Field and their cars must be flagged to indicate Provisional driver to other drivers.

Failure to attend any nominated meeting, without reasonable notification to LCA, will result in RoF starts for all races at the next nominated meeting.

Nomination acceptance – any drivers nomination may be declined for ANY event, including state and national titles if it is deemed that the drivers inclusion may cause a negative impact on the event(s). This decision must be submitted to the Legend Cars Australia Director to ensure no bias or personal grudges affect the decision.

## TITLE NOMINATIONS

To be eligible to compete in a National Title, State Titles or money (Blue Ribbon) events, a Driver must have competed in a minimum of two regular championship rounds (not titles), or more ***at the discretion of LCA***, in the current season or previous 12 months in the Racing Division.

***At the discretion of the LCA***, dispensation may be provided if the Driver has driven in an affiliated class or a Racing Division determined by the Organising Body as providing requisite experience, in the current season or previous 12 months.

The previous year's National Champion and current Australian Title holder shall be permitted to participate in the Australian title event, regardless of the above prerequisites.

***No late nominations for Title meetings will be accepted.***

## **TRANSPONDERS**

All transponders are to be mounted on left or right hand side rail behind tow hook with direct line of sight to the ground (front of car). Incorrectly mounted transponders will result in data not being transmitted to timing system causing a non-classified result.

MyLaps is now in situ at many tracks and transponders are sometimes available for hire at those tracks. However, there is no guarantee MyLaps transponders will be available on any given day. Failure to have a transponder fitted at timed events, will be deemed the responsibility of the driver and that car will not be recorded in official results and no points will be allocated for that event. Transponders may be hired directly by contacting MyLaps for periods of 12 months and more.

**Correct mounting of transponder is the driver's responsibility.**

## **SERIES SPONSOR**

Please welcome our new Series Sponsor, Simpson Toorace Australia. All visor decal requirements are to be met at all times. Any car not displaying the appropriate decal will not be eligible for points and may not be permitted to race until decal rectified.



## **ACCOUNTS PAYABLE**

Contact Tony Ward on 0419 564 595 or email [info@legendcarsaustralia.com.au](mailto:info@legendcarsaustralia.com.au). No credit will be extended unless an Account application has been approved. No further credit will be extended to accounts exceeding 60 days. LCA reserves the right to suspend any current registered car associated with a driver, owner or associate who does not pay their account in a timely manner. Late fees will be applied at 10% of outstanding amount per month on overdue accounts. By accepting credit from LCA, you agree to these terms.

**All Legend Cars and Legend Car parts remain the property of LCA until full and final payment is made, including any outstanding late fees applied.**

No other driver will be permitted to participate in any Legend Car event in that car, until the account is settled.

If a car is sold with outstanding amounts owed to LCA, that car will not be permitted to race, nor will it be registered, until full payment has been received by LCA. It is the responsibility of the purchaser to ensure the car is unencumbered at the time of sale. Tony Ward can be contacted on 0419 564 5953 for all account queries.

Any suspensions will be indefinite from all LCA events until the account is paid in full. The participant will be notified in writing of the suspension. When the account is paid in full, the suspended participant/car will be permitted to compete.

Personal cheques written to LCA with insufficient funds will result in immediate suspension throughout the entire series until the debt is cleared.



2021/2022 & 2022/2023 Australian Title Winner – Robert Rawlings







# LEGEND CARS AUSTRALIA

## 23/24 DIRT CALENDAR

ALL DATES SUBJECT TO CHANGE



| DATE                        | VENUE                             | EVENT                          |
|-----------------------------|-----------------------------------|--------------------------------|
| Saturday, 23 September 2023 | TOOWOOMBA SPEEDWAY - QLD          | NRQC RD 1                      |
| Saturday, 7 October 2023    | MARYBOROUGH SPEEDWAY - QL         | NRQC RD 2                      |
| Saturday, 14th October 2023 | MANJIMUP SPEEDWAY - WA            | WAC RD 1                       |
| Saturday, 21 October 2023   | GOULBURN SPEEDWAY - NSW           | SRC RD 1                       |
| Saturday, 28 October 2023   | COLLIE SPEEDWAY -WA               | WAC RD 2                       |
| Saturday, 11 November 2023  | BUNBURY SPEEDWAY - WA             | WAC RD 3                       |
| Saturday, 18 November 2023  | LISMORE SPEEDWAY - NSW            | NRQC RD 3                      |
| Saturday, 18 November 2023  | NARROGIN SPEEDWAY - WA            | WAC RD 4                       |
| Friday, 1 December 2023     | KWINANA MOTOPLEX - WA             | WAC RD 5                       |
| Saturday, 2 December 2023   | ACT SPEEDWAY                      | SRC RD 2                       |
| Saturday, 9 December 2023   | NOWRA SPEEDWAY - NSW              | SRC RD 3                       |
| Saturday, 9 December 2023   | MT BARKER SPEEDWAY -WA            | WAC RD 6                       |
| Saturday, 16 December 2023  | COLLIE SPEEDWAY - WA              | WAC RD 7                       |
| Tuesday 26 December 2023    | BUNDABERG SPEEDWAY - QLD          | NRQC RD 4                      |
| Wednesday, 27 December 2023 |                                   |                                |
| Saturday, 30 December 2023  | MARYBOROUGH SPEEDWAY - QL         | NRQC RD 5                      |
| Saturday, 30 December 2023  | NOWRA SPEEDWAY - NSW              | SRC RD 4                       |
| Saturday, 6 January 2024    | SAPPHIRE SPEEDWAY - NSW           | SRC RD 5                       |
| Friday, 12 January 2024     | KWINANA MOTOPLEX - WA             | WAC RD 8                       |
| Saturday, 13 January 2024   | <b>NOWRA SPEEDWAY - NSW TITLE</b> | <b>NSW TITLE</b>               |
| Friday, 19 January 2024     | GRAFTON SPEEDWAY - NSW            | NRQC RD 6/7                    |
| Saturday, 20 January 2024   | <b>DOUBLE HEADER</b>              |                                |
| Saturday, 3 February 2024   | <b>ACT SPEEDWAY - ACT TITLE</b>   | <b>ACT TITLE</b>               |
| Saturday, 3 February 2024   | BUNBURY SPEEDWAY - WA             | WAC RD 9                       |
| Saturday, 10 February 2024  | BUNDABERG SPEEDWAY - QLD          | NRQC RD 8                      |
| Saturday, 17 February 2024  | COLLIE SPEEDWAY - WA              | WAC RD 10                      |
| Saturday, 2 March 2024      | <b>BUNBURY SPEEDWAY - WA TITL</b> | <b>WA TITLE</b>                |
| Saturday, 9 March 2024      | ACT SPEEDWAY                      | SRC RD 6                       |
| Saturday, 16 March 2024     | <b>TOOWOOMBA SPEEDWAY - QLD</b>   | <b>QLD TITLE</b>               |
| Friday, 22 March 2024       | KWINANA MOTORPLEX - WA            | WAC RD 11                      |
| Saturday, 23 March 2024     | GOULBURN SPEEDWAY - NSW           | SRC RD 7                       |
| Friday, 29 March 2024       | BUNBURY SPEEDWAY - WA             | WAC RD 12                      |
| Friday, 29 March 2024       | ROMA SPEEDWAY - QLD               | NRQC RD 3/10                   |
| Saturday, 30 March 2024     | <b>DOUBLE HEADER</b>              |                                |
| Saturday, 30 March 2024     | MARGARET RIVER SPEEDWAY - WA      | WAC RD 13                      |
| Sunday, 31 March 2024       | <b>DOUBLE HEADER</b>              |                                |
| Saturday, 30 March 2024     | NOWRA SPEEDWAY - NSW              | SRC RD 8                       |
| Saturday, 6 April 2024      | ACT SPEEDWAY                      | SRC RD 9                       |
| Saturday, 13 April 2024     | LOCKYER VALLEY SPEEDWAY - QLD     | NRQC RD 11                     |
| Saturday, 13 April 2024     | COLLIE SPEEDWAY - WA              | WAC RD 14                      |
| Saturday, 27 April, 2024    | SAPPHIRE SPEEDWAY - NSW           | SRC RD 10                      |
| Saturday, 18 May 2024       | SAPPHIRE SPEEDWAY - NSW           | SRC RD 11                      |
| Saturday, 25 May 2024       | GRAFTON SPEEDWAY - NSW            | NRQC RD 12                     |
| Saturday, 1 June 2024       | MORUYA SPEEDWAY - NSW             | SRC RD 12                      |
| Saturday, 8 June 2024       | LISMORE SPEEDWAY - NSW            | <b>23/24 AUSTRALIAN TITLES</b> |
| Sunday, 9 June 2024         | <b>AUSTRALIAN TITLES</b>          |                                |

# R.E.

**AUTHORIZED DEALER**



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